

# Welcome to the Departures Presentation for the Montlake Elementary School Modernization and Addition Project

Seattle Public Schools

July 2022





# Land Acknowledgment

*We would like to show our respect and acknowledge the Puget Sound Coast Salish peoples, past and present, on whose lands we gather today. The Suquamish Tribe and Muckleshoot Indian Tribe are the federally recognized Indian tribes of greater Seattle, under the treaties of Point Elliott and Medicine Creek.*



## Departures Presentation Summary

The Seattle Land Use Code does not include a “school zone.” Most schools are located in single family residential zones and often do not meet the underlying zoning requirements. Therefore, public schools can request exemptions, known as departures, from the land use code.

This document presents the Departures from the City of Seattle Land Use Code sought by Seattle Public Schools for the construction of the Montlake Elementary School Modernization and Addition project.

The following departures will allow the project to fulfill the community-developed vision and Seattle Public Schools’ requirements within the site constraints described within this presentation.

# Requested Departures Summary

## 1. Departure for Lot Coverage: SMC 23.51B.002.C.2 and 3.

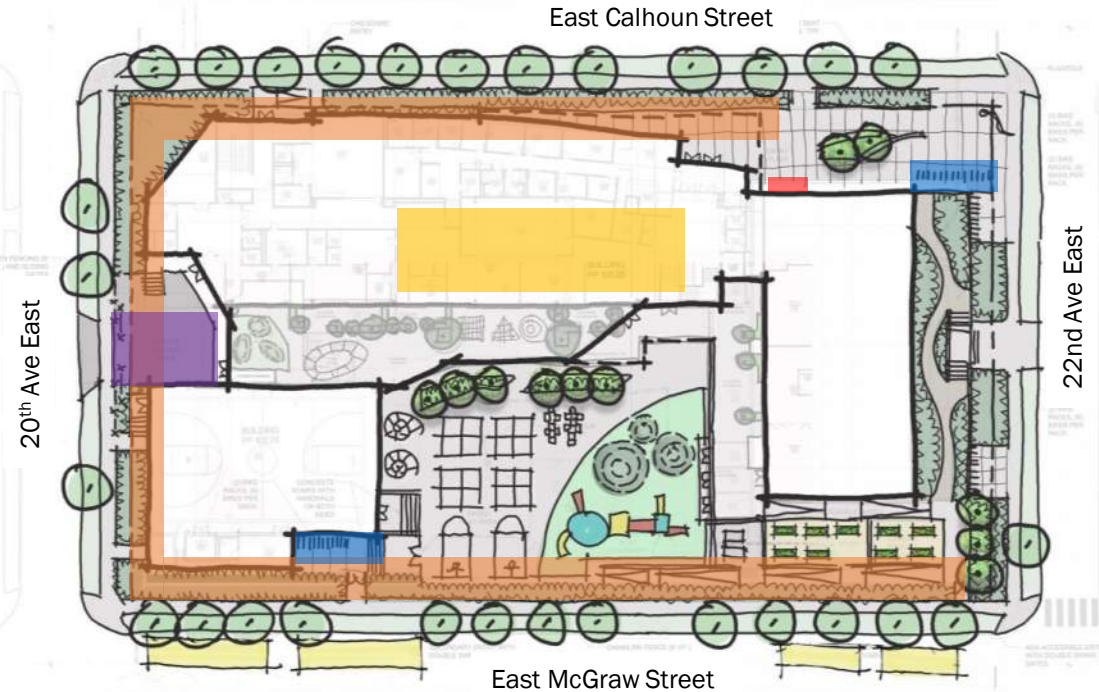
For additions to existing public school structures on existing public school sites, the maximum lot coverage is the greater of existing lot coverage or 35% of the lot area for structures more than one story. Departures from lot coverage limits may be granted per Chapter 23.79 for up to 45% lot coverage for structures more than one story. Lot coverage restrictions may be waived by the Director as a Type I decision when waiver would contribute to reduced demolition of residential structures. Seattle Public Schools (SPS) proposes a lot coverage of 57%.


## 2. Departure for Building Height: SMC 23.51B.002.D.1.c.

The code allows a maximum building height that is the greater of the existing school height or 35' above average grade plane. SPS proposes an additional 15' above the existing building height in limited areas to accommodate mechanical and elevator equipment penthouses.

## 3. Departure for Setbacks: SMC 23.51B.002, Table E

The code requires 15 foot setbacks for façade heights of 35 to 50 feet for additions on an existing public school site located across a street or alley from lots in a residential zone. SPS proposes setbacks of 3 feet along the north, 6 feet along the west, and 5 feet along the south property lines.



-  Building Height
-  Setbacks
-  Bicycle Parking
-  Changing Image Message Board Sign
-  Bus/Truck Loading/Unloading

# Requested Departures Summary

**4. Departure for Vehicular Parking Quantity: SMC 23.54.015, Table C**

The proposed design would require 131 on-site vehicle parking spaces per code. SPS proposes 0 automobile parking spaces on-site.

**5. Departure for Bus & Truck Loading/Unloading: SMC 23.51B.002.I.2**

The Seattle Land Use Code requires one off-street truck loading berth that is 13 feet wide and 40 feet long for new public school construction. SPS proposes one 25 foot wide loading berth that is 34'-0" in depth from the property line.

**6. Departure for Curb Cut to Service Area without Parking: SMC 23.54.030.F.2.b.3**

For public schools, the maximum width of a curb cut is 25 feet. Development standard departures may be granted or required pursuant to the procedures and criteria set forth in Chapter 23.79. SPS proposes one new 25 foot wide curb cut along 20th Ave E to access a proposed service (loading and solid waste hauling) area without parking spaces.

**7. Departure for Bicycle Parking (Long Term) Quantity: SMC 23.54.015 Table D**

The Seattle Land Use Code requires 55 long-term bicycle parking spaces for the proposed design. SPS proposes 30 long-term bicycle parking spaces for a departure of 25 long-term bicycle parking spaces.

**8. Departure for Bicycle Parking Standards: SMC 23.54.015.K.2.**

The Seattle Land Use Code requires secure locations for all long-term bicycle parking with features such as locked rooms or cages and bicycle lockers. Seattle Public Schools proposes a secured enclosure for 12 of the 30 long-term spaces provided and open, weather protected bike racks for 18 long-term bike parking spaces.

**9. Departure for Changing Image Message Board Sign: SMC 23.55.020**

The Seattle Land Use Code does not allow flashing, changing-image or message board signs in single-family zones. SPS proposes one changing image message board sign.



## Project Scope

Montlake Elementary School Modernization and Addition:

Modernize the existing Landmarked building and construct a new 65,000 square foot Pre-K through 5<sup>th</sup> Grade addition to the existing building for a capacity of 500 students to address projected growth in Seattle Public Schools. Six existing portable classroom structures, one existing cafeteria/lunchroom structure, and one existing greenhouse structure will be demolished to accommodate the new addition.

Anticipated Start of Construction: Summer 2023

Anticipated Occupancy: Fall 2025

# Presentation Contents

## Departures Process Overview

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## Requested Departures

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3. Setbacks
4. Vehicular Parking Quantity
5. Bus/Truck Loading & Unloading – Loading Berth
6. Curb Cut to Service Area without Parking
7. Bicycle Parking (Long Term) Quantity
8. Bicycle Parking Standards
9. Electric Changing Image Message Board Sign

## Public Comments and Contact



# Departures Process Overview

Process Changes Due to Covid-19  
Purpose and Intent  
Evaluation Criteria  
Recommendations  
Public Comment + Contact



# Process Changes Due to COVID-19

The school departure recommendation process typically requires in-person public meetings, which are prohibited due to public health mandates on social distancing and limited gatherings.

While this ordinance is in effect, DON staff will accept written public comment and the Director of Seattle Department of Neighborhoods (DON) will make a recommendation to the Seattle Department of Construction and Inspections (SDCI), taking into consideration the public's comments, in lieu of the committee holding public meetings.

Due to public health mandates on social distancing and limited public gatherings related to COVID-19, the Seattle City Council approved, and Mayor Durkan signed, Ordinance 126188 in October 2020.

The ordinance allows certain City land use processes to be handled administratively for the duration of the COVID-19 civil emergency declared for the City of Seattle in March 2020.

Thus, the DON Director is temporarily authorized to submit this recommendation report to SDCI in lieu of a public advisory committee process. The content of the report is informed by public comments solicited and reviewed by DON staff.



# Purpose and Intent

Most schools are in single family zone neighborhoods, the land use code does not include a “school zone”

Renovation and additions often will not meet the underlying zoning; therefore, the public schools can request exemptions, known as departures, from the land use code.

This process is an opportunity for neighbors and the surrounding community to give the City feedback on the requested departures.

At this time, the Director of Department of Neighborhoods (DON), taking into consideration public comment, can recommend to grant, grant with condition, or deny the requested departures.



# Evaluation Criteria – Consistency

(SMC 23.79.008)

Departures shall be evaluated for consistency with the general objectives and intent of the City's Land Use Code, including the rezone evaluation criteria in Chapter 23.34 of the Seattle Municipal Code, to ensure that the proposed facility is compatible with the character and use of its surroundings.



# Evaluation Criteria – Relationship

(SMC 23.79.008)

In reaching recommendations, the Director of Department of Neighborhoods (DON) shall consider and balance the interrelationships among the following factors:

**Relationship to Surrounding Areas.** The Director of Department of Neighborhoods (DON) shall evaluate the acceptable or necessary level of departure according to:

- 1) Appropriateness in relation to the character and scale of the surrounding area;
- 2) Presence of edges (significant setbacks, major arterials, topographic breaks, and similar features) which provide a transition in scale;
- 3) Location and design of structures to reduce the appearance of bulk;
- 4) Impacts on traffic, noise, circulation and parking in the area; and
- 5) Impacts on housing and open space. More flexibility in the development standards may be allowed if the impacts on the surrounding community are anticipated to be negligible or are reduced by mitigation; whereas, a minimal amount or no departure from development standards may be allowed if the anticipated impacts are significant and cannot be satisfactorily mitigated.

# Evaluation Criteria – Need

(SMC 23.79.008)

**Need for Departure.** The physical requirements of the specific proposal and the project's relationship to educational needs shall be balanced with the level of impacts on the surrounding area. Greater departure may be allowed for special facilities, such as a gymnasium, which are unique and/or an integral and necessary part of the educational process; whereas a lesser or no departure may be granted for a facility which can be accommodated within the established development standards.



# Recommendations

Recommendations must include consideration of the interrelationship among height, setback and landscaping standards when departures from height or setback are proposed.

# Public Comment + Contact

Thank you for taking the time to review this document.

Please submit your comments on the requested departures, including any mitigation measures or conditions of approval by September 9, 2022 to:

Nelson Pesigan

[Nelson.pesigan@seattle.gov](mailto:Nelson.pesigan@seattle.gov)

City of Seattle, Department of Neighborhoods

Attn. Nelson Pesigan

PO Box 94649

Seattle, WA, 98124-4649



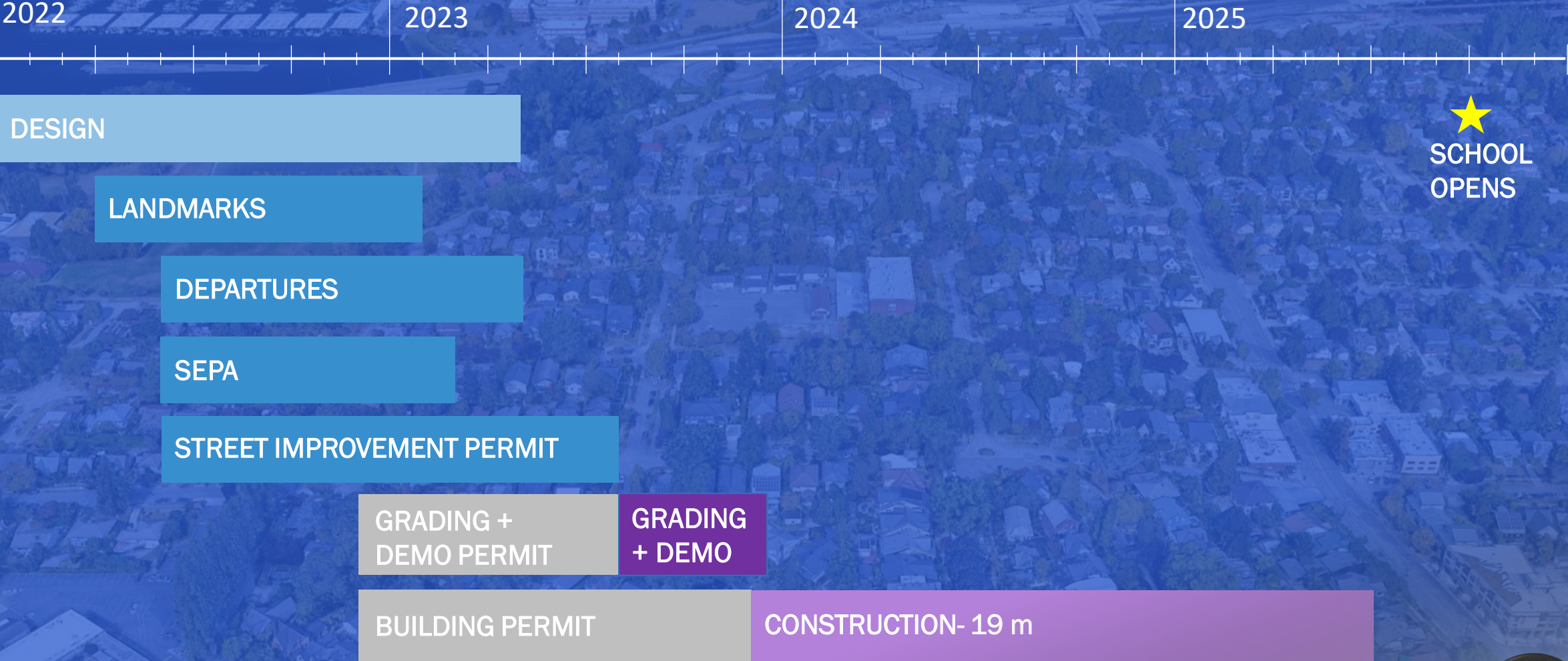
# Project Overview

Schedule  
Design Review Agencies and Processes  
SDAT and Community Engagement  
Project Vision  
Site Context and Analysis



# Montlake Elementary School Modernization and Addition

## Project Schedule





# Montlake Elementary School Modernization and Addition

## Design Review Process

The Project Team is working with many stakeholders throughout the design of John Rogers Elementary School Replacement including, but not limited to, the following:

### City Agencies

Seattle Department of Construction and Inspections (SDCI)

Seattle Department of Transportation (SDOT)

Department of Neighborhoods (DON)

King County - Public Health

Seattle Public Utilities (SPU)

Seattle City Light (SCL)

School Traffic Safety Committee

### School District Departments

School Board

Teaching and Learning

Capital Projects

Operations

Maintenance

Department of Technology Services

Risk Management

Legal

# Montlake Elementary School Modernization and Addition

## School Design Advisory Team (SDAT)

The Project Team engaged with a team of teachers, parents and neighbors through a series of eight workshops, over the course of six months, in developing the project's goals, vision and preliminary building and site adjacencies. The following individuals participated in the Montlake Elementary School Modernization and Addition SDAT :

**Julia Pearson**, Principal

**Paul Wight**, SPS

**Jack Marshall**, Teacher

**Beau Browman**, Teacher

**Jennifer Lundgren**, Teacher

**Rebecca Gerben Mehta**, Parent

**Melissa (Missy) Pody**, Montlake Staff  
and Parent

**Anny Hunt**, Parent

**Julie Choung**, Parent

**Ewa Sack**, Parent

**Mary Beth Hribar**, Parent

**Sarah Bruemmer**, Parent

**Bianca DiJulio**, Parent

**Cathy Patnoe**, Special Education Teacher

**Chester Weir**, Parent

**Rob Dolin**, Parent

**Matthew Sneddon**, Parent

**Tim Moore**, Teacher

**Jessica Kaarbo**, Parent, Teacher  
at Bertschi School

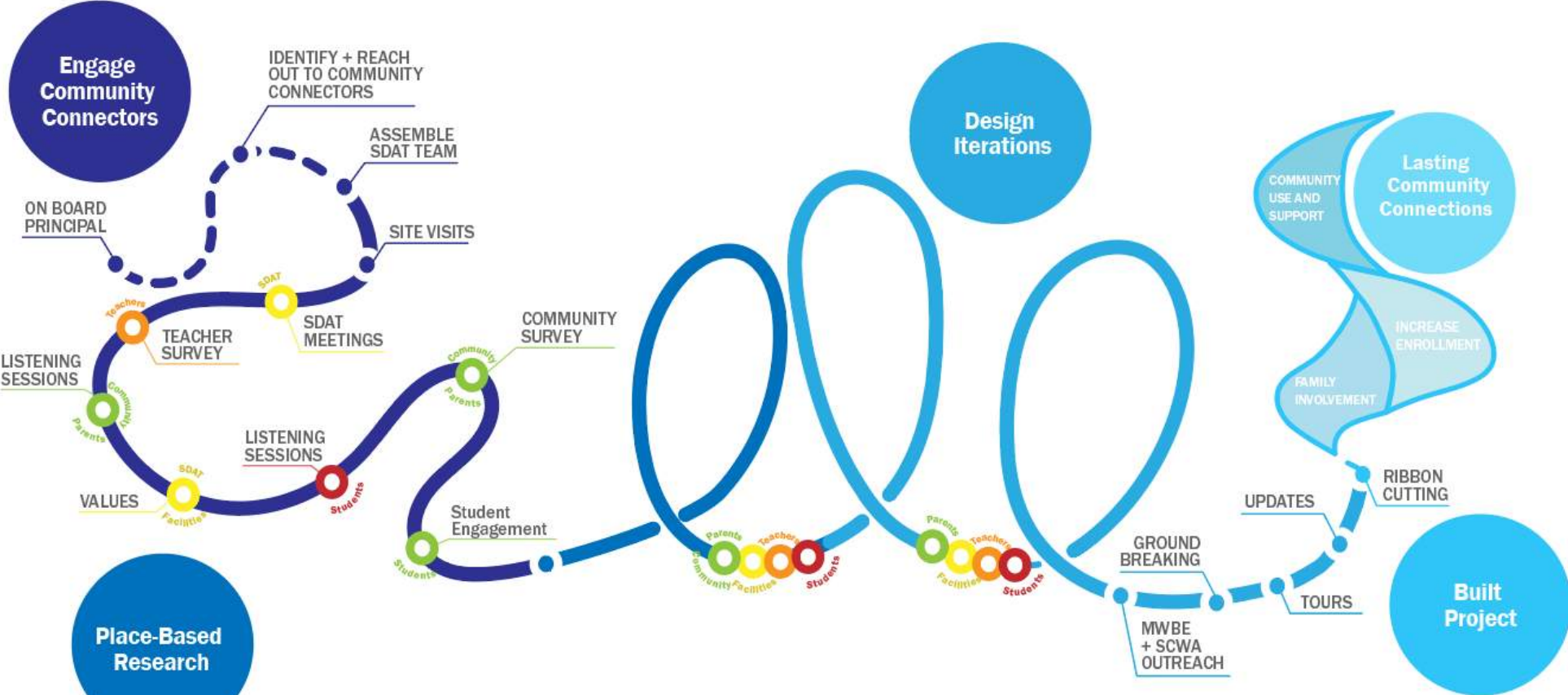
**Cheri Bloom**, Neighbor, former  
garden educator



# Montlake Elementary School Modernization and Addition

## Community Engagement

Through both the Pre-Design and Schematic Design phases of the project, the design team worked to understand priorities, concerns, hopes and desires from the broad community surrounding Montlake Elementary School.



# Montlake Elementary School Modernization and Addition

## Project Vision and Goals

Through community listening sessions, student and teacher input, community surveys and the SDAT process, a Project Vision and guiding direction for the project was established. The project team incorporated reference to Montlake's beloved garden program in the definition of the Project Vision:

### A Verdant Garden of Change-makers

*A place to create, cultivate and connect*

We will grow the change-makers of tomorrow through hands-on learning that cultivates in students the skills and passion/confidence to make a positive difference in the world.



#### Inclusive Neighborhood Network

- Safety and Accessibility
- Neighborhood Partnerships and Engagement



#### Spirit of Place

- Honor the Landscape
- Context and Culture



#### Holistic Health

- Connection to Nature
- Comfort and Well-being
- Inclusivity



#### Cultivating a Resilient Future

- Resource Conservation
- Resilience



# Site Context

## Vicinity

Montlake is located in central Seattle along the Montlake Cut of the Lake Washington Ship Canal. It is bounded to the north by Portage Bay, to the east by the Washington Park Arboretum, and to the south and west by Interlaken Park. Capitol Hill is on its south and west sides, and the University of Washington campus lies across the Montlake Cut to the north. State Route 520 runs through the northern tip of Montlake.

Current enrollment at Montlake Elementary School is 182 students.



■ Student Catchment Area

# Site Context

## Spirit of Place

The project team engaged with members of the Muckleshoot Tribe to provide context and perspective on the specific importance of the Montlake area to the indigenous peoples of the Puget Sound. Near the current path of State Route 520 was a primary native canoe portage route between Lake Washington and Lake Union. In 1916, the Montlake Cut was constructed as a shipping channel to connect the two lakes. The cut had far-reaching implications disrupting areas of cultural importance, native fishing habitats and the ecology of the entire region.





# Site Context

## Transportation

The area is served by public transportation and a strong network of pedestrian and cycle pathways that cross through the neighborhood, one of them being the Neighborhood Greenway, which runs directly along the Montlake Elementary School site.



-  City Bus
-  Neighborhood Greenway
-  Montlake Cut Connector Trail



# Site Analysis

## Montlake Elementary School – Existing Site

Montlake Elementary School sits on a 1.65 acre site located at the center of the Montlake neighborhood. It is bounded by E Calhoun St. to the north, E McGraw St. to the south, 20<sup>th</sup> Ave. E to the west and 22<sup>nd</sup> Ave. E to the east. The surrounding area is zoned SF5000 and consists of mostly single-family homes with some low-density retail located on 24<sup>th</sup> Ave. Despite the limited site area, the school grounds feature a small garden, greenhouse and outdoor classroom that are treasured features for the school community.





# Site Analysis

## Tree Canopy

Zooming into the city block scale, reveals the extensive tree canopy, shown in green. The Montlake neighborhood is notable for the mature trees that line the streets and create a sense of scale and character throughout the seasons.




# Site Analysis

## Existing Site Plan

The current site is a double block that includes the existing historic school building, six portable classrooms, a detached cafeteria building, and a play area with hard surface play and a small area of turf with play equipment. The main school building was designed by Seattle architect Floyd Naramore and constructed in 1924. The main school building including attached play court/boiler/gym structure is landmarked by City of Seattle.

 Landmarked Building

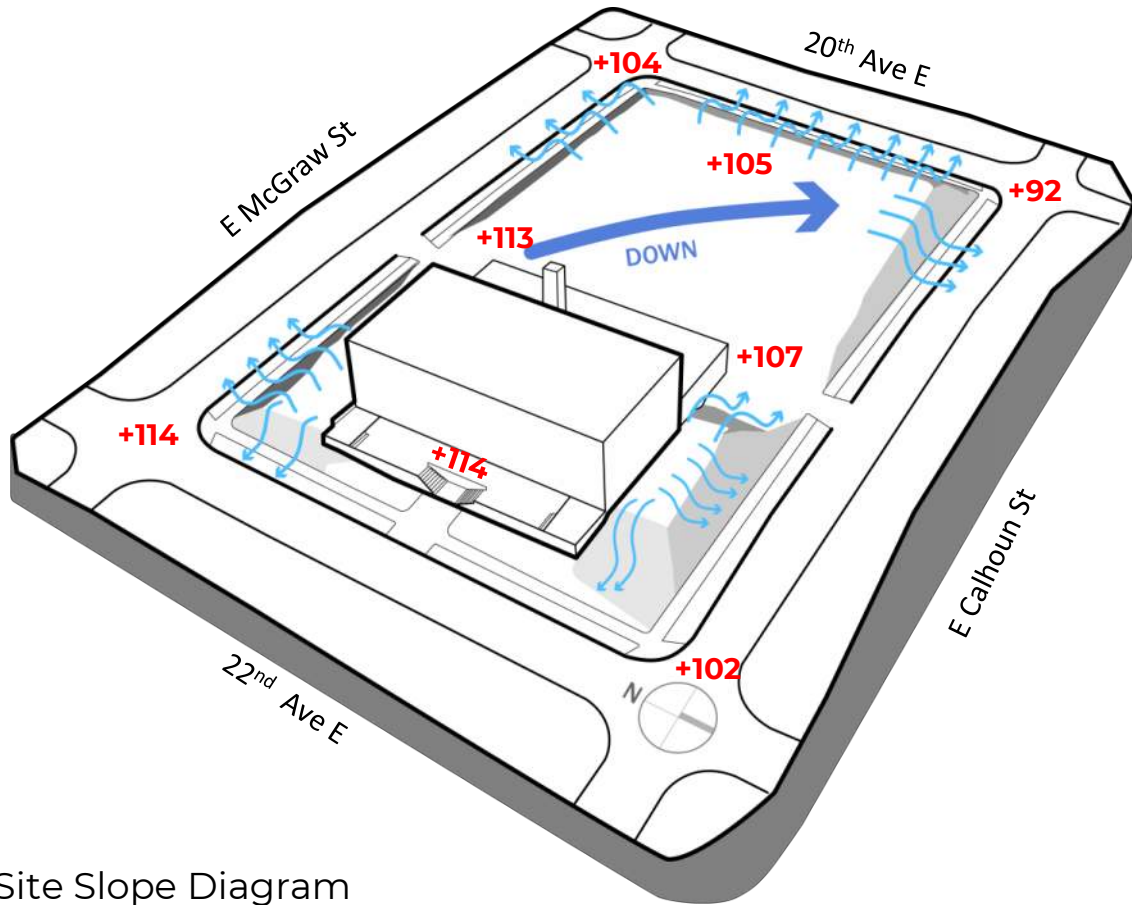
 Non-Contributing





# Site Analysis | Existing Site Slopes

The existing site slopes in two directions with the high point at the corner of 22<sup>nd</sup> Ave E and E McGraw St. and a low point at 20<sup>th</sup> Ave. E and E Calhoun St. In addition to the general site slope, the original school is built on a raised area with additional raised berms located along the site edges.



Site Slope Diagram





# Site Analysis | Existing Building



Existing Landmarked Building from corner of 22<sup>nd</sup> Ave. E and E Calhoun St.



# Proposed Design

- Site Plan
- Building
- Materials

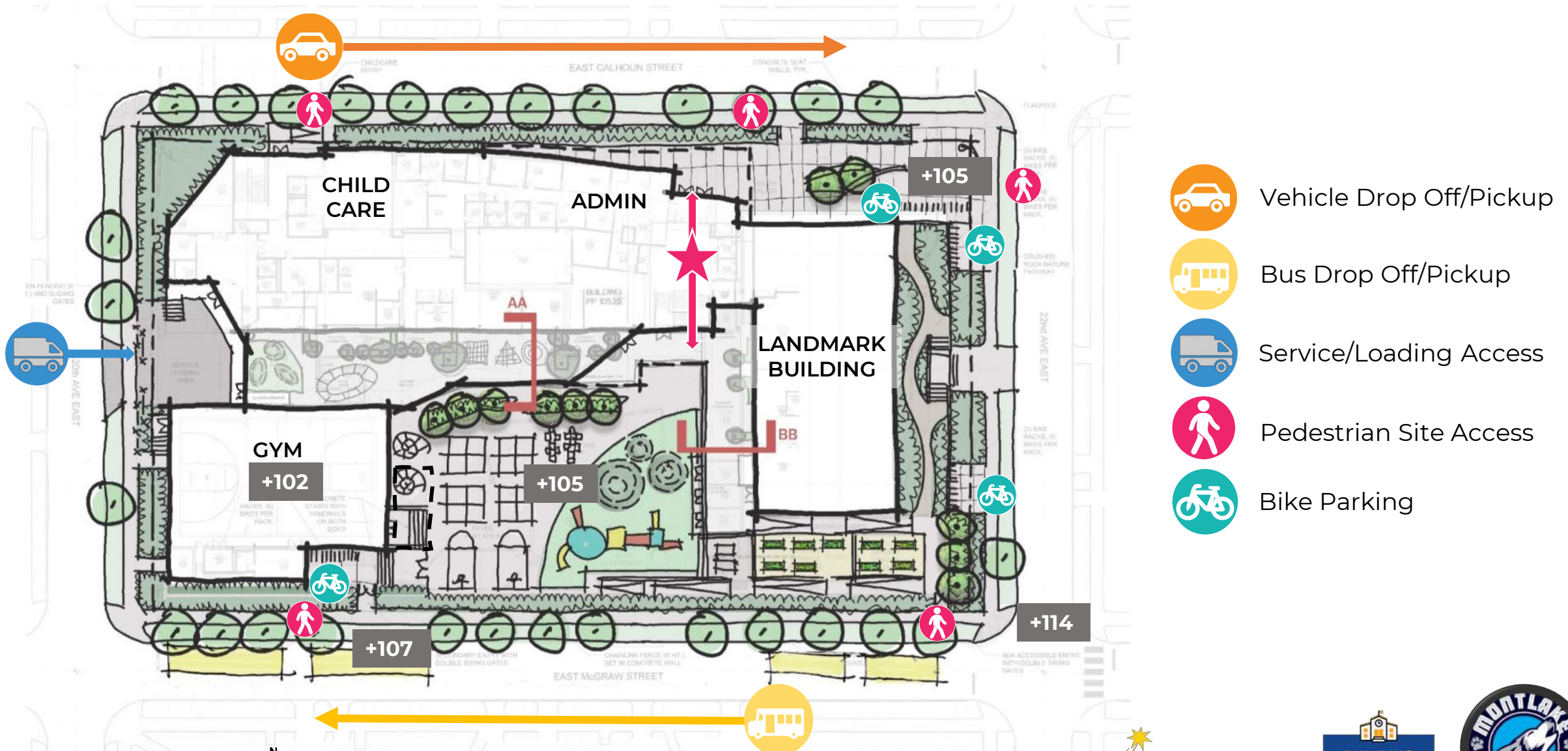


# Proposed Site Plan





# Proposed Site Plan | Site Access

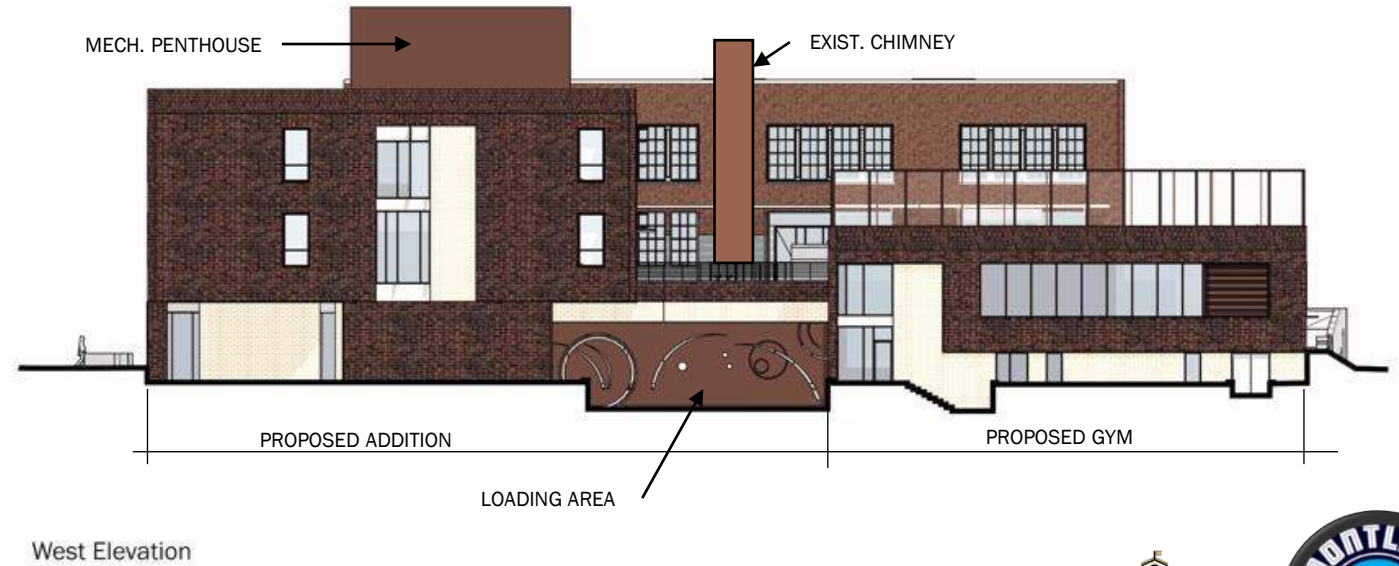


# Proposed Building | Elevations

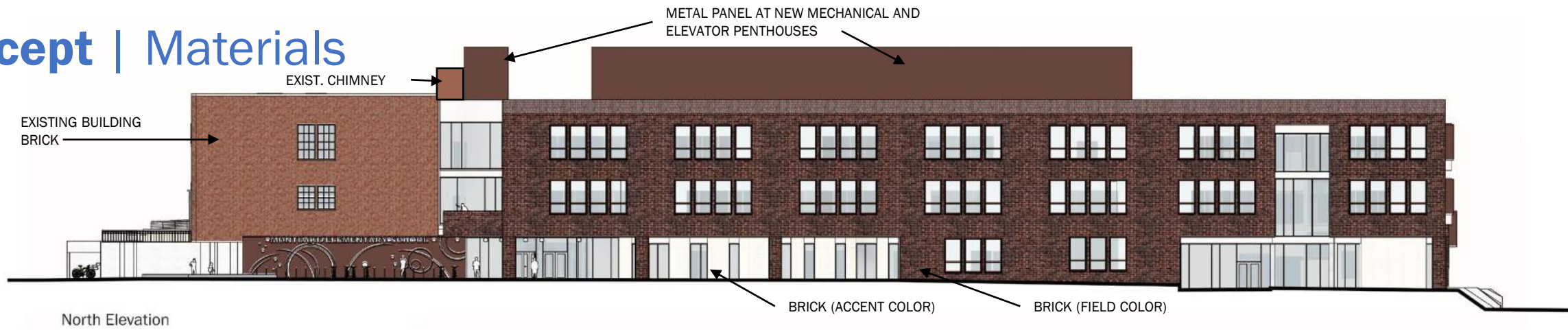




# Proposed Building | Elevations



# Concept | Materials



BRICK (FIELD COLOR)



BRICK (ACCENT COLOR)



DARK METAL PANEL COLOR



# Requested Departures

1. Lot Coverage
2. Building Height
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4. Vehicular Parking Quantity
5. Bus/Truck Loading & Unloading – Loading Berth
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# Requested Departure #1: **Lot Coverage**

# Requested Departure #1 : Lot Coverage

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
SMC 23.51B.002.C. – Lot Coverage in Single Family Zones

C.2. For new public school construction and additions to existing public school structures on existing public school sites, the maximum lot coverage permitted is the greater of the following:

- a. The lot coverage permitted in 23.51B.002.C.1 (35% of lot area for structure more than one story); or
- b. The lot coverage of the former school structures on the site...

C.3. Departures from lot coverage limits may be granted or required pursuant to the procedures and criteria set forth in Chapter 23.79. Up to 45% lot coverage may be allowed for structures of more than one story. Lot coverage restrictions may be waived by the Director as a Type I decision when waiver would contribute to reduced demolition of residential structures.

**Departure Requested:** Seattle Public Schools requests a Departure to allow 45% lot coverage. (In addition, SPS requests that the director waive lot cover restrictions as allowed by SMC to permit an additional 12% for a total of 57% lot coverage.)

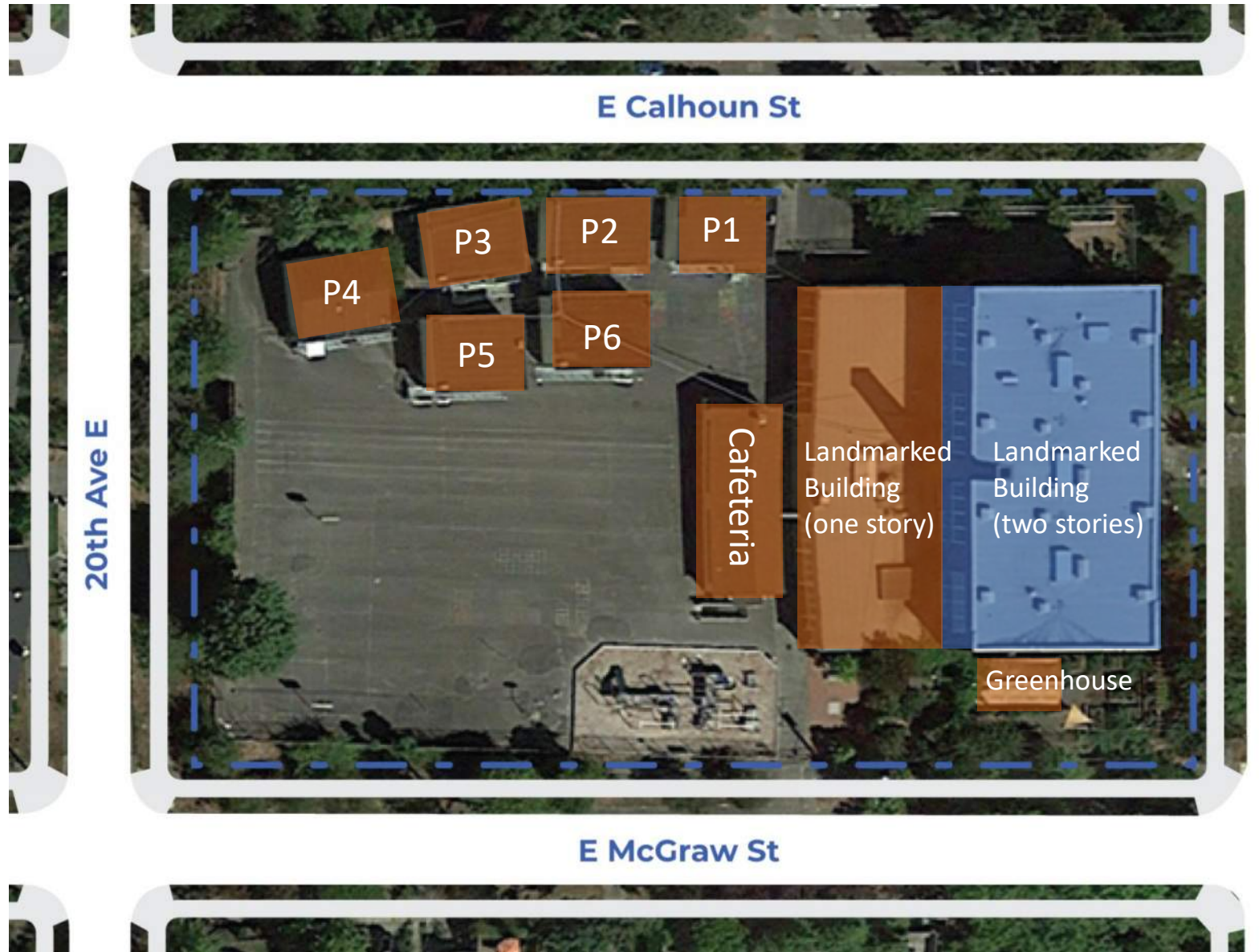


# Requested Departure #1 : Lot Coverage

## EXISTING LOT COVERAGE

<u>Space</u>	<u>Square Feet</u>
Existing Building	16,943
Cafeteria	1,563
Greenhouse	483
Portable P1	793
Portable P2	784
Portable P3	784
Portable P4	890
Portable P5	784
Portable P6	782
Total Building Area	23,806
Total Site Area	72,000
Existing Lot Coverage	33%

Existing Lot Coverage is less than allowable by SMC 23.51B.002.C.1



Building to Remain



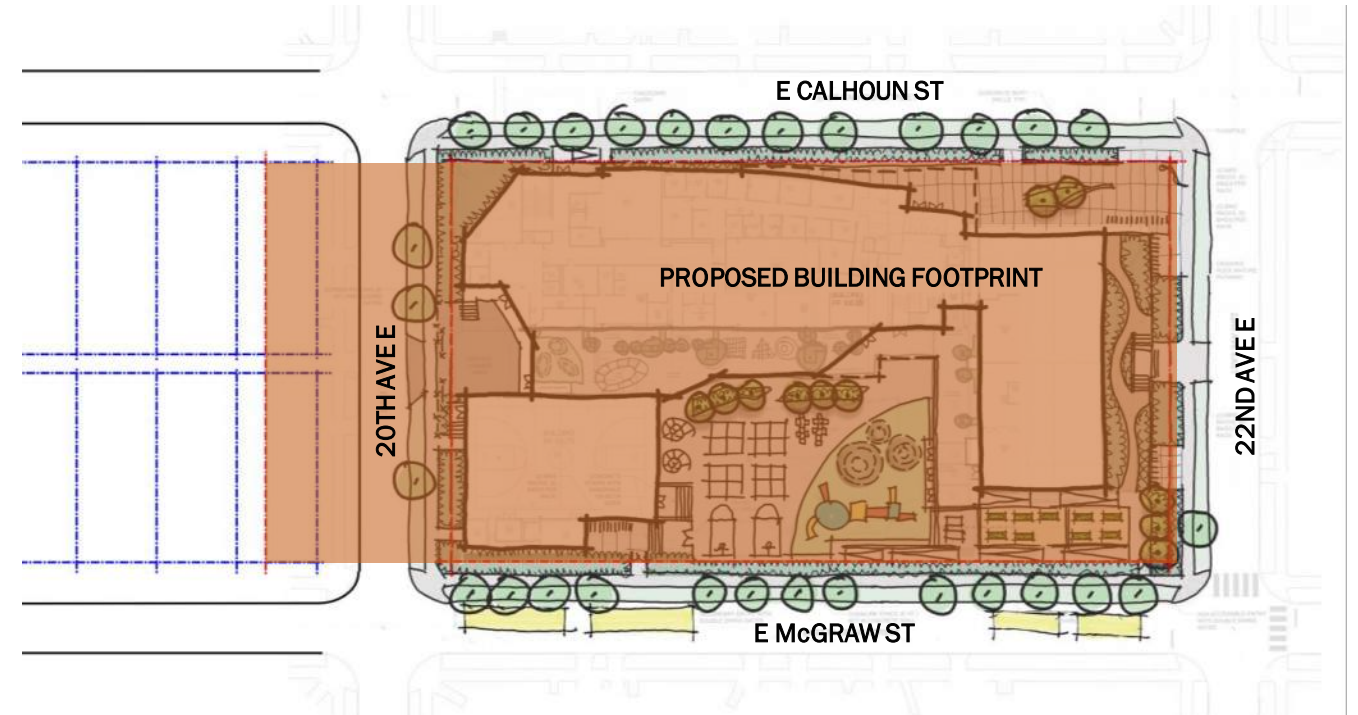
Proposed for Demolition


# Requested Departure #1 : Lot Coverage

## 45% ALLOWABLE LOT COVERAGE

This graphic shows property extents that would be required to accommodate school program within a 45% lot coverage. An additional 18,500 square feet of property would need to be annexed in order to meet this lot coverage threshold and would require vacating a portion of 20<sup>th</sup> Ave E between E Calhoun and E McGraw Streets, as well as condemning and demolishing two residential properties across 20<sup>th</sup>.

SPS requests a departure to allow 45% lot coverage and that the Director waive lot coverage restrictions to avoid demolition of residential property.



 Site area required to accommodate 45% lot coverage

 Property Line

Proposed building footprint = 40,700 SF  
Total site area = Existing + Additional = 90,435 SF  
Lot Coverage with additional area = 45%



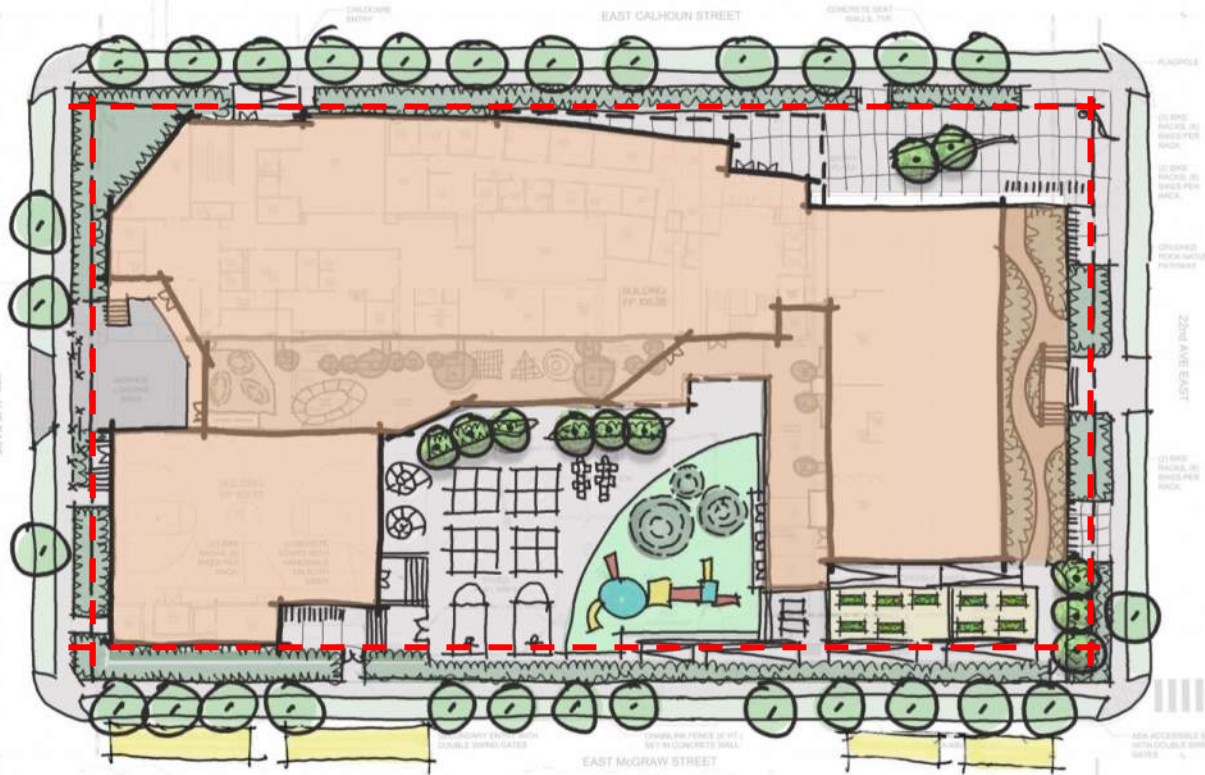
# Requested Departure #1 : Lot Coverage

*Consistency:* The proposed Montlake Elementary School lot coverage has been designed to meet Seattle Public Schools educational program, while minimizing building mass to remain consistent with the intent of the code.

*Relationship:* The proposed design has been created to maximize outdoor play area as well as mitigate building height in a way that is appropriate to the scale and character of the surrounding neighborhood. The design also takes advantage of existing mature tree cover to reduce the perceived scale of the building.

*Need:* The Montlake Elementary School site area is the second smallest in the District, at 72,000 SF or 1.65 acres. This small site area limits the available site square footage to accommodate program square footage without demolishing residential structures.

In order to satisfy the program requirements, Seattle Public Schools proposes a total of 57% lot coverage.



Proposed building footprint

Property Line

Proposed building footprint = 40,700 SF

Total site area = 72,000 SF

Proposed Lot Coverage = 57%





# Requested Departure #1 : Lot Coverage



Perspective View from 22<sup>nd</sup> Ave. E and E McGraw St.



## Departure #1 Summary

For additions to existing public school structures on existing public school sites the maximum lot coverage is the greater of existing lot coverage or 35% of the lot area for structures more than one story. Departures from lot coverage limits may be granted per Chapter 23.79 for up to 45% lot coverage for structures more than one story. Lot coverage restrictions may be waived by the Director as a Type I decision, when waiver would contribute to reduced demolition of residential structures.

Seattle Public Schools requests a Departure to allow 45% lot coverage. (In addition, SPS requests that the director waive lot cover restrictions as allowed by SMC to permit an additional 12% for a total of 57% lot coverage.)



# Requested Departure #2: **Building Height**



# Requested Departure #2 : Building Height

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
SMC 23.51B.002.D. – Height

D.1.c. For additions to existing public schools on existing public school sites, the maximum height permitted is the height of the existing school or 35 feet plus 15 feet for a pitched roof, whichever is greater. When the height limit is 35 feet, the ridge of the pitched roof on a principal structure may extend up to 15 feet above the height limit, and all parts of the roof above the height limit must be pitched at a rate of not less than 4:12. No portion of a shed roof is permitted to extend beyond the 35 foot limit under this provision

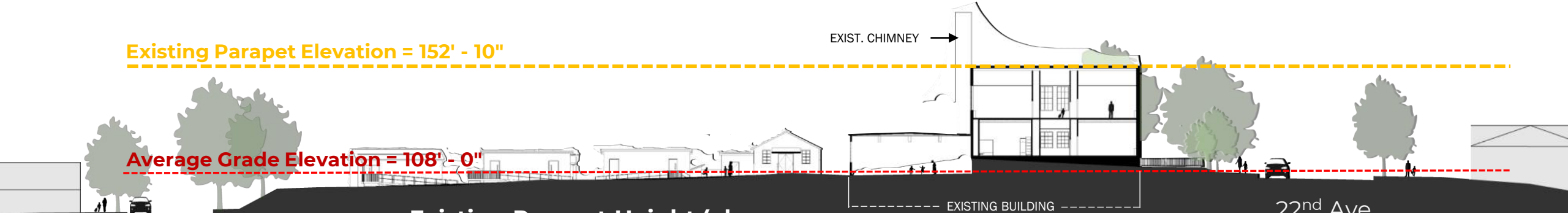
Departure Requested: Seattle Public Schools requests a Departure of 15 feet above the existing building height, measured from average grade level, to accommodate a mechanical and elevator equipment penthouses on the new addition.

# Requested Departure #2 : Building Height

Existing Height – Site Sections



North – South Section



**Existing Parapet Height (above average grade) = 44'-10"**

Existing Chimney Height = 20' above existing building height

East – West Section




# Requested Departure #2 : Building Height

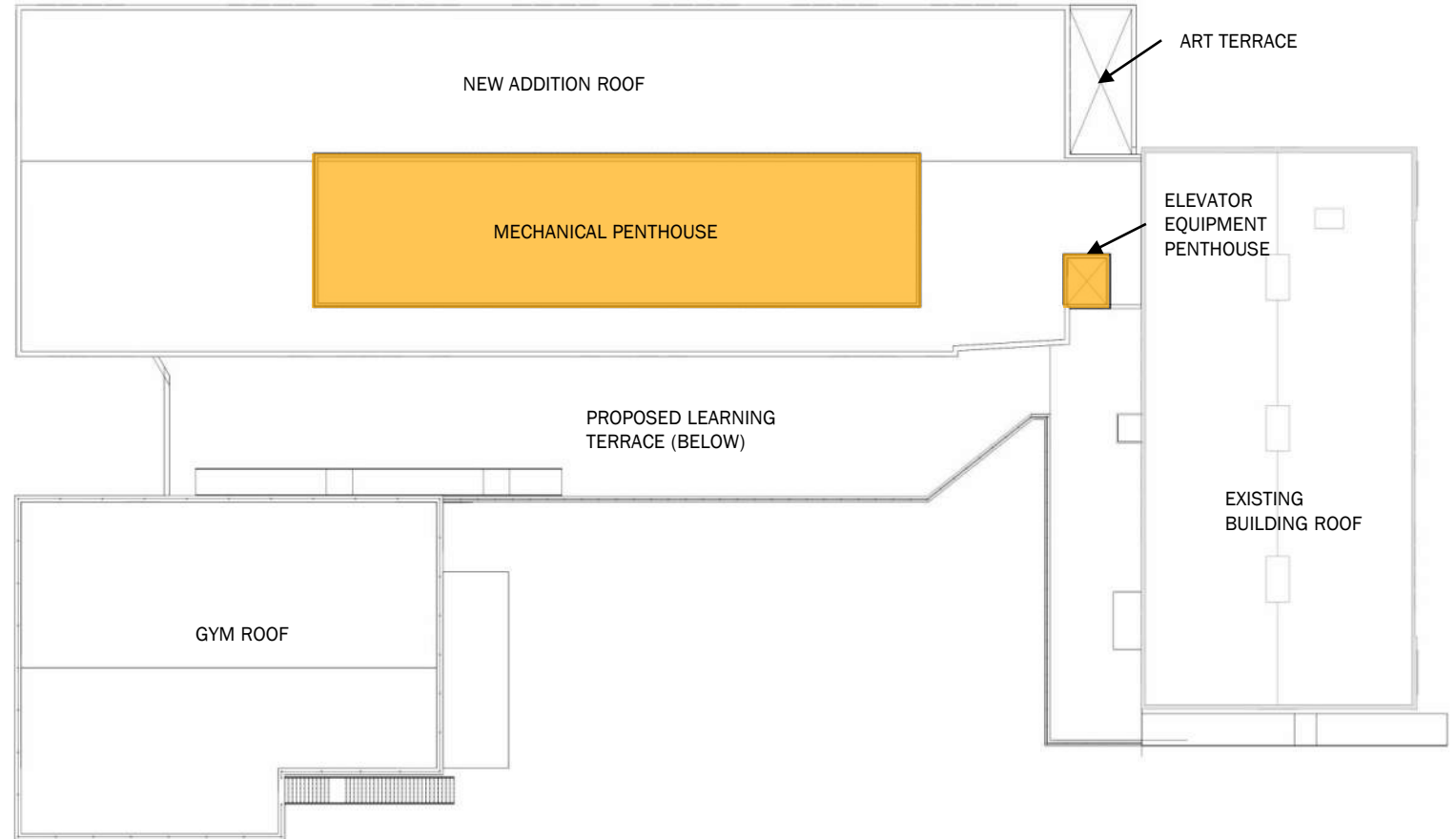
Existing Building Height = 44' - 10"

Existing Building Height is greater than 35' height noted in SMC 23.51B.002.D.1.c, therefore allowable height is 44'-10" above average grade.

Design proposes to exceed the existing building height in order to provide penthouses for mechanical and elevator equipment.

Areas exceeding the existing building height = 11.4% of total roof area

 Proposed roof area of requested Departure



## Requested Departure #2 : Building Height

*Consistency:* The proposed Montlake Elementary School has been designed to minimize building height to remain consistent with the intent of the code.

*Relationship:* The proposed design maintains the parapet line of the existing school and is in keeping with the established scale of the site. A mechanical penthouse and elevator equipment penthouse have been sized and located to minimize visibility from pedestrians on the sidewalk.

*Need:* The Montlake Elementary School needs to be able to accommodate the mechanical equipment that provides heating, cooling and fresh air to students as well as an elevator to provide accessible travel through the building. The requested penthouses allow this equipment to function properly without taking program space out of the main building volumes.

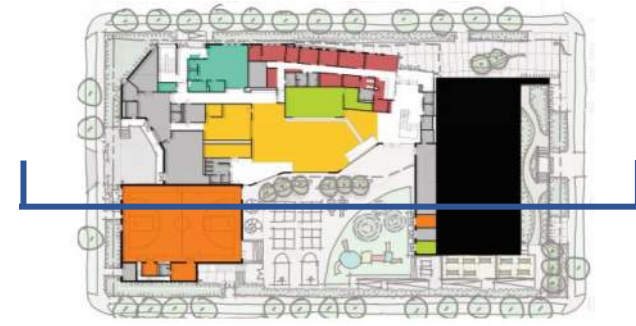
In order to satisfy functional requirements, Seattle Public Schools proposes a building height of not more than 60' at the mechanical and elevator penthouses.





# Requested Departure #2 : Building Height

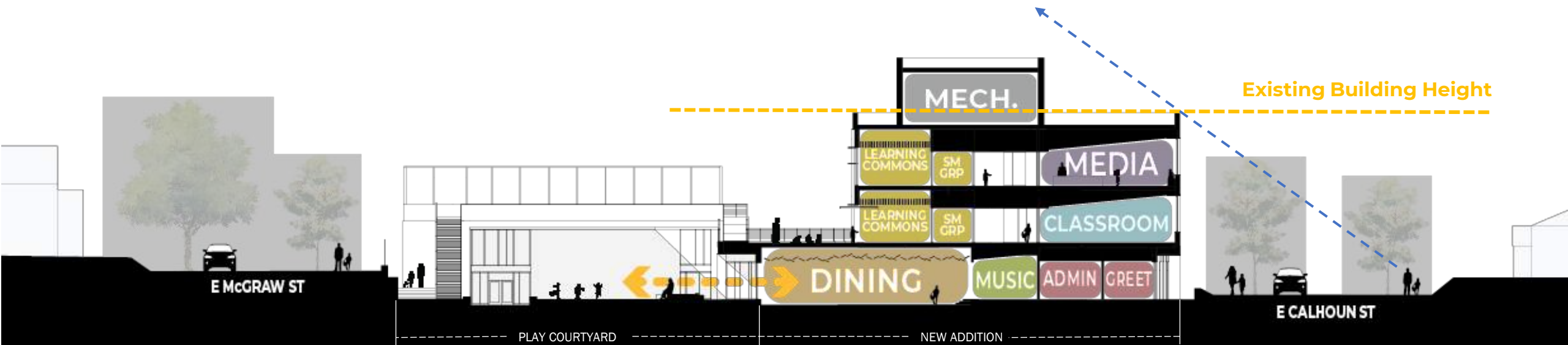
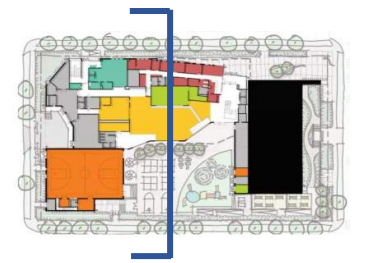
The proposed addition uses the height of the existing historic building as the governing height for the new addition. The addition and courtyard play space are cut down into the site to provide required program square footages while keeping the addition height and floor levels in alignment with the existing building. Limited areas will exceed this height to accommodate mechanical and elevator penthouses.



East – West Section

# Requested Departure #2 : Building Height

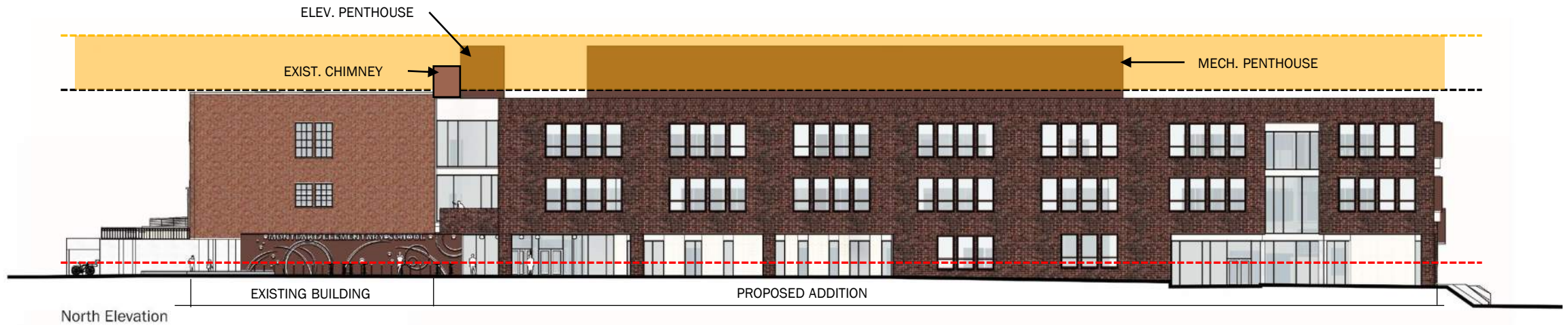
Limited areas will exceed the existing building height for mechanical and elevator penthouses but will be set back from the building edge to decrease visibility from pedestrians on the sidewalk.



North – South Section



# Requested Departure #2 : Building Height



- Average Existing Grade
- Existing School Height Above Average Existing Grade
- Requested Departure Height of 15' Above Maximum Building Height Allowed by Zoning Code (for mechanical and elevator penthouses\*)

Area above existing building height; select portions of building only. Set back approximately 37' from E Calhoun Street property line.

\* Mechanical and elevator penthouses are enclosures expressly for the purpose of housing mechanical and elevator equipment. These enclosures provide weather protection to prolong the life of the equipment, visual barriers, and acoustic buffers to mitigate equipment noise.



# Requested Departure #2 : Building Height



View from across E Calhoun St. showing pedestrian perspective



## Departure #2 Summary

The Seattle Land Use Code permits additions to existing public schools on existing public school sites to be the height of the existing school or 35 feet plus 15 feet for a pitched roof, whichever is greater.

SPS proposes a departure of 15' above the existing building height to accommodate mechanical and elevator penthouses.



# Requested Departure #3: **Setbacks**



# Requested Departure #3 : Setbacks

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
SMC 23.51B.002.E. – Setbacks

E.4.a. For additions to existing public school structures on existing public school sites across a street or alley from lots in residential zones shall provide either the setback of the previous structure on the site or 15 foot setbacks for façade heights greater than 35 feet up to 50 feet measured from average grade level:

Departure Requested: Seattle Public Schools proposes setbacks of 3 feet along the north property line, 6 feet along the west property line, and 5 feet along the south property line, for a departure of 12 feet along the north property line, 9 feet along the west property line, and 10 feet along the south property line.

# Requested Departure #3 : Setbacks

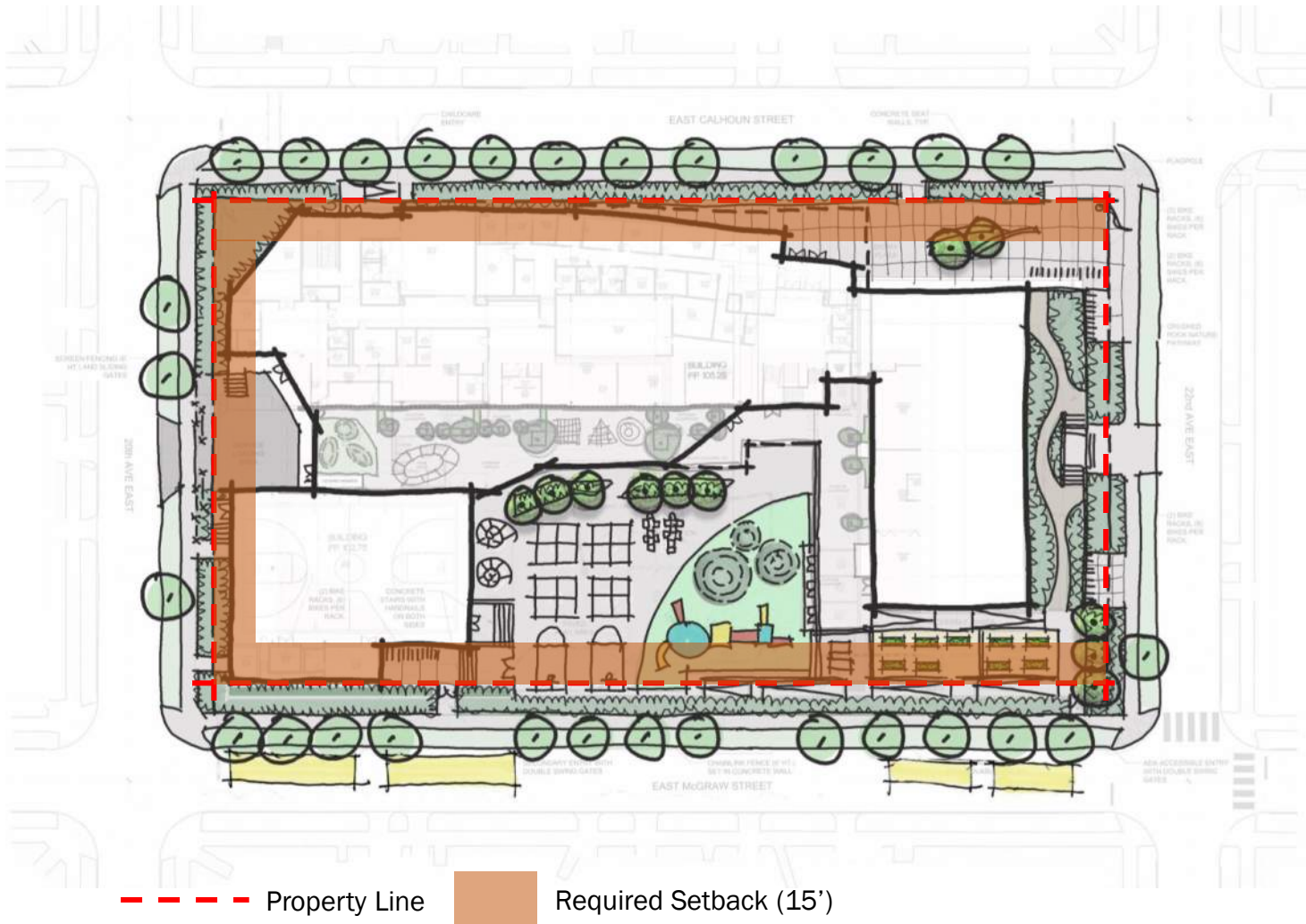
*Consistency:* The proposed Montlake Elementary School has been designed to meet Seattle Public Schools educational program, while providing setback to the greatest extent possible to remain consistent with the intent of the code.

*Relationship:* The existing property line is 6'-6" from the back of sidewalk consistently around the site. While the SMC requires the setback to be calculated from property line, the actual distance from the sidewalk to building face will be greater. This additional distance will make the perceived setback more generous and soften the transition in scale.

*Need:* The Montlake Elementary School site area is the second smallest in the District, at 72,000 SF or 1.65 acres. This small site area limits the available site square footage to accommodate program square footage without further increase to building height.

Seattle Public Schools proposes the following setbacks:

- 3 feet along the North property line (9'-6" from sidewalk)
- 6 feet along the West property line (12'-6" from sidewalk)
- 5 feet along the South property line (11'-6" from sidewalk)





## Departure #3 Summary

The code requires 15 foot setbacks for façade heights of 35 to 50 feet for additions on an existing public school site located across a street or alley from lots in a residential zone.

Seattle Public Schools proposes setbacks of 3 feet along the north property line, 6 feet along the west property line, and 5 feet along the south property line, for a departure of 12 feet along the north property line, 9 feet along the west property line, and 10 feet along the south property line.





# Requested Departure #4: Vehicular Parking Quantity



# Requested Departure #4 : Vehicular Parking Quantity

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
 SMC 23.51B.002.G. – Parking Quantity  
 SMC 23.54 – Quantity and Design Standards for Access, Off-Street Parking, and Solid Waste Storage.  
 SMC 23.54.015 Table C – Required Parking for Public Uses and Institutions

Required Parking:

Childcare Calculation

1 Parking Space per 10 Children  $30/10 =$  3 spaces

Childcare Loading Zone Calculation

1 Loading Zone Space per 20 Children  $30/20 =$  2 spaces

Elementary School Calculation

Public Assembly Spaces at 80 spaces/SF  
 Dining Commons + Gymnasium  $10,086/80 =$  126 spaces


**Total: 131 spaces**

Departure Requested: Required Parking Spaces 131 Spaces  
 Provided Parking Spaces 0 Spaces  
 Departure: 131 Spaces

# Requested Departure #4 : Vehicular Parking Quantity



## Existing On-Site Parking Quantity

 \*Existing On-Street Parking (not included in parking calculation): RPZ 1 Permit required.

Per SMC 23.51B.002.G, the existing school building would require 25 automobile parking spaces. 0 designated parking spaces are currently provided.

There is currently a small paved service area off of E Calhoun St where one or two staff members park infrequently, however this area does not contain marked parking spaces as defined in the Land Use Code.





# Requested Departure #4 : Vehicular Parking Quantity

*Consistency:* The code required number of parking spaces is derived from the number of spaces needed during a large assembly event, rather than a more typical day-to-day need. An on-street parking availability study, performed by Heffron Transportation, Inc., demonstrates that on-street parking capacity can support the code required parking demand for large assembly events if certain transportation and parking recommendations are implemented. The transportation study determined there are 598 on-street parking spaces within the existing study area (within 800-foot walking distance from the site), and these remained under the rate at which the City of Seattle considers full capacity (85% of available spots filled) during all time periods studied. The proposal is not anticipated to place undue stress on the existing neighborhood parking.

*Relationship:* The proposed number of parking spaces is less than the code required number in order to accommodate the Seattle Public Schools educational program requirements and to maximize the amount of play and outdoor learning opportunities on the site. The square footage required to provide the desired educational program and maximize outdoor play areas further limit available parking areas on-site. Additionally, the School Design Advisory Team (SDAT) and results from Community Surveys, reinforced the desire that the project provide outdoor play and learning opportunities that supported both the school and broader neighborhood community.

*Need:* The Montlake Elementary School site area is the second smallest in the District, at 72,000 SF or 1.65 acres. This small site area limits the available site square footage to accommodate the addition to the existing school to meet the current Seattle Public Schools educational program requirements, as well as the desire to maximize outdoor play. Subsequently, the available square footage for on-site parking is also limited. Parking quantities that meet the Seattle Municipal Code could not reasonably be achieved without drastically reducing available student program space and site amenities.



# Requested Departure #4 : Vehicular Parking Quantity

## Previous Seattle Public Schools Departures:

Due to educational program space requirements in the SPS Standards, most recently constructed elementary schools have provided less on-site parking area than what is required by code in order to preserve educational program space.

School Name	Enrollment Capacity	Site Area (Acres)	On-Site Parking Provided/Required	Departure
Arbor Heights Elementary	650	5.65	55 / 138	80
Genesee Hill Elementary	650	6.82	71 / 135	64
Loyal Heights	650	2.85	0 / 70	70
Magnolia Elementary	500	2.50	6 / 79	73
Queen Anne Elementary	500	3.00	32 / 118	86
Thornton Creek Elementary	650	7.66	91 / 162	71
Wing Luke Elementary	500	6.85	60 / 130	70
Kimball Elementary	650	4.78	40 / 140 (proposed)	100
Northgate Elementary	650	5.77	30 / 140 (proposed)	110
Montlake Elementary	500	1.65	0 / 131 (proposed)	131

# Requested Departure #4 : Vehicular Parking Quantity

## Transportation and Parking Impacts Analysis Summary:

### Elements Evaluated:

- **Replacement school** – Up to 500 students in grades K-5 plus 30-student childcare classroom (planned for before- and after-school care, but could accommodate added pre-school students). Evaluated potential of up to 530 students in Pre-K-5 (an increase of 343 students compared to 2022 enrollment)
- **Number & time of added school trips** – Morning Arrival (7:30-8:30 a.m.) increase = 238 trips (134 in, 104 out); Afternoon Dismissal (2:15-3:15 p.m.) increase = 152 trips (71 in, 81 out).
- **Traffic operations & safety** – Eight intersections; all forecast to operate at Level of Service (LOS) D or better overall. No significant adverse impacts to study area traffic operating conditions. Collision data did not indicate any unusual collision patterns.
- **On-street parking** – Daily use & evening events. City considers occupancy of 85% or higher as effectively full. Currently, 60% to 64% occupied on school days (201 to 242 unused). With project, school-days to remain between 65% and 71%. During most events (~1 per month) expected to remain below 85%; largest event (e.g., Curriculum Night) could cause full parking or demand beyond 800 feet—measures recommended.
- **Construction traffic & Mitigation** – Reviewed and approved by SDOT for construction access and haul routes to minimize negative impacts on the surrounding neighborhood.

Source: *Montlake Elementary School Modernization and Addition Transportation Technical Report*, Heffron Transportation, Inc., 2022.



# Requested Departure #4 : Vehicular Parking Quantity

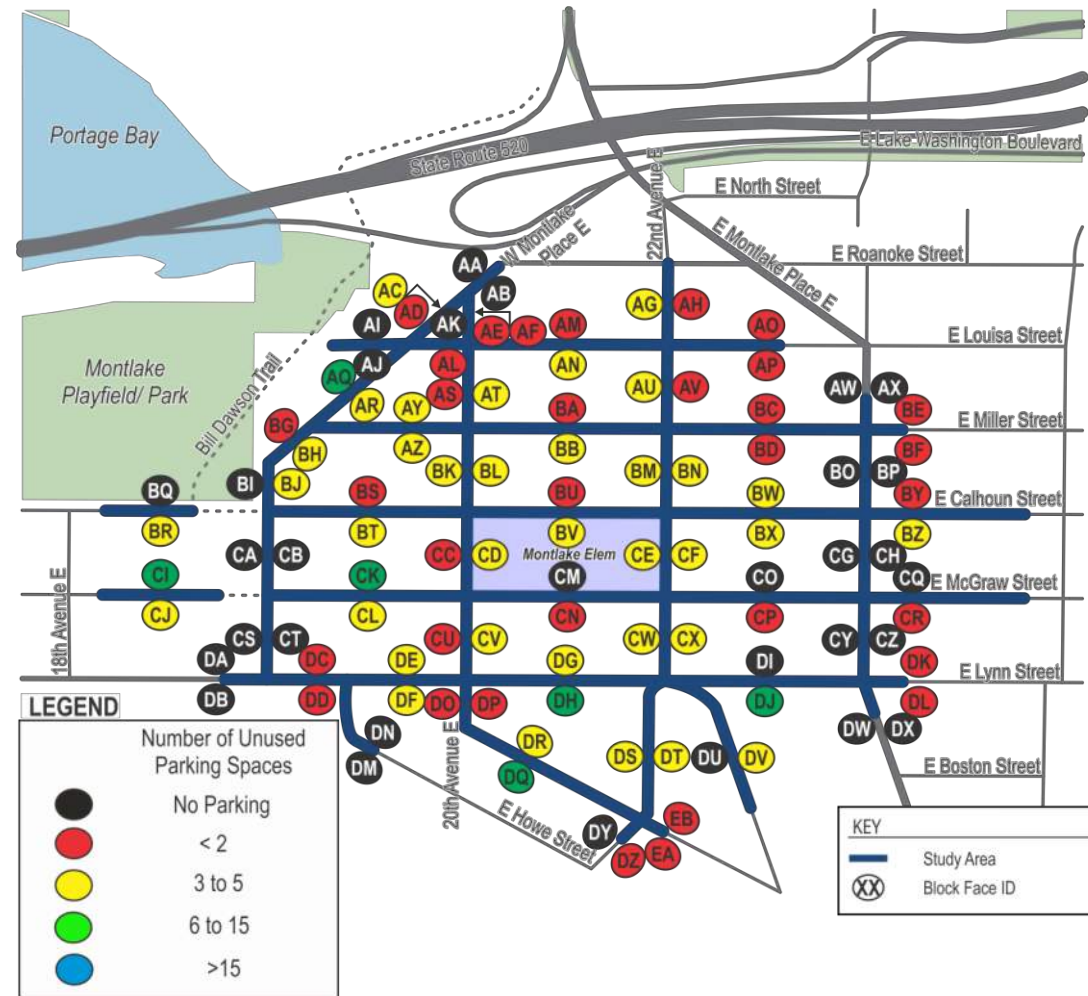
## On-Street Parking Occupancy - Weekday Early Morning

Total On-Street Parking Spaces within 800-ft walking distance = 575 Spaces

- 360 to 374 cars (7:00 - 7:45 a.m.) (63% to 65% occupied)\*
- Number Unoccupied = 201 to 215

\* Note: City of Seattle considers occupancy rates of 85% or higher to be effectively full.

Source: *Montlake Elementary School Modernization and Addition Transportation Technical Report*, Heffron Transportation, Inc., 2022.



# Requested Departure #4 : Vehicular Parking Quantity

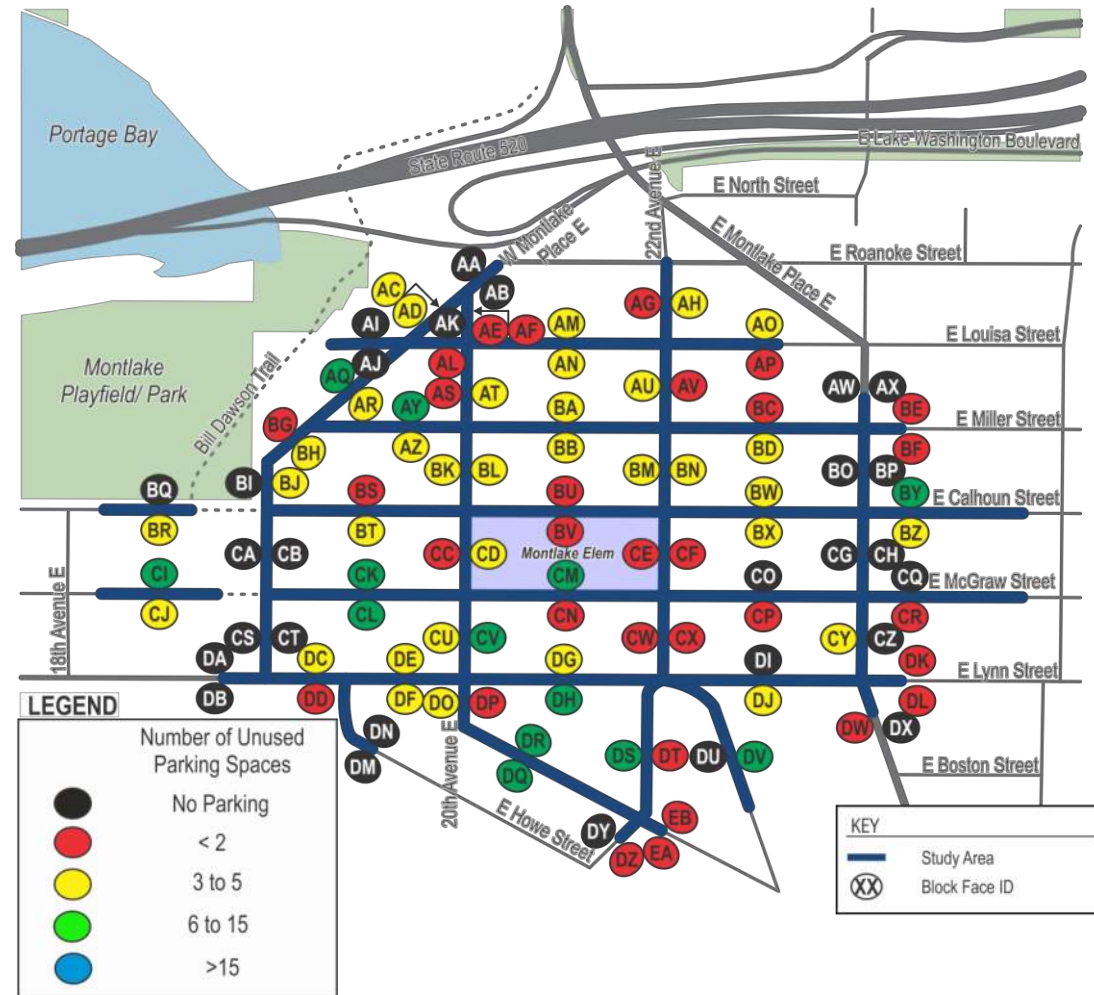
## On-Street Parking Occupancy - Weekday Mid Morning

Total On-Street Parking Spaces within 800-ft walking distance = 598 Spaces

- 356 to 358 cars (10:30 - 11:15 a.m.) (60% occupied)\*
- Number Unoccupied = 240 to 242

\* Note: City of Seattle considers occupancy rates of 85% or higher to be effectively full.

Source: *Montlake Elementary School Modernization and Addition Transportation Technical Report*, Heffron Transportation, Inc., 2022.





# Requested Departure #4 : Vehicular Parking Quantity

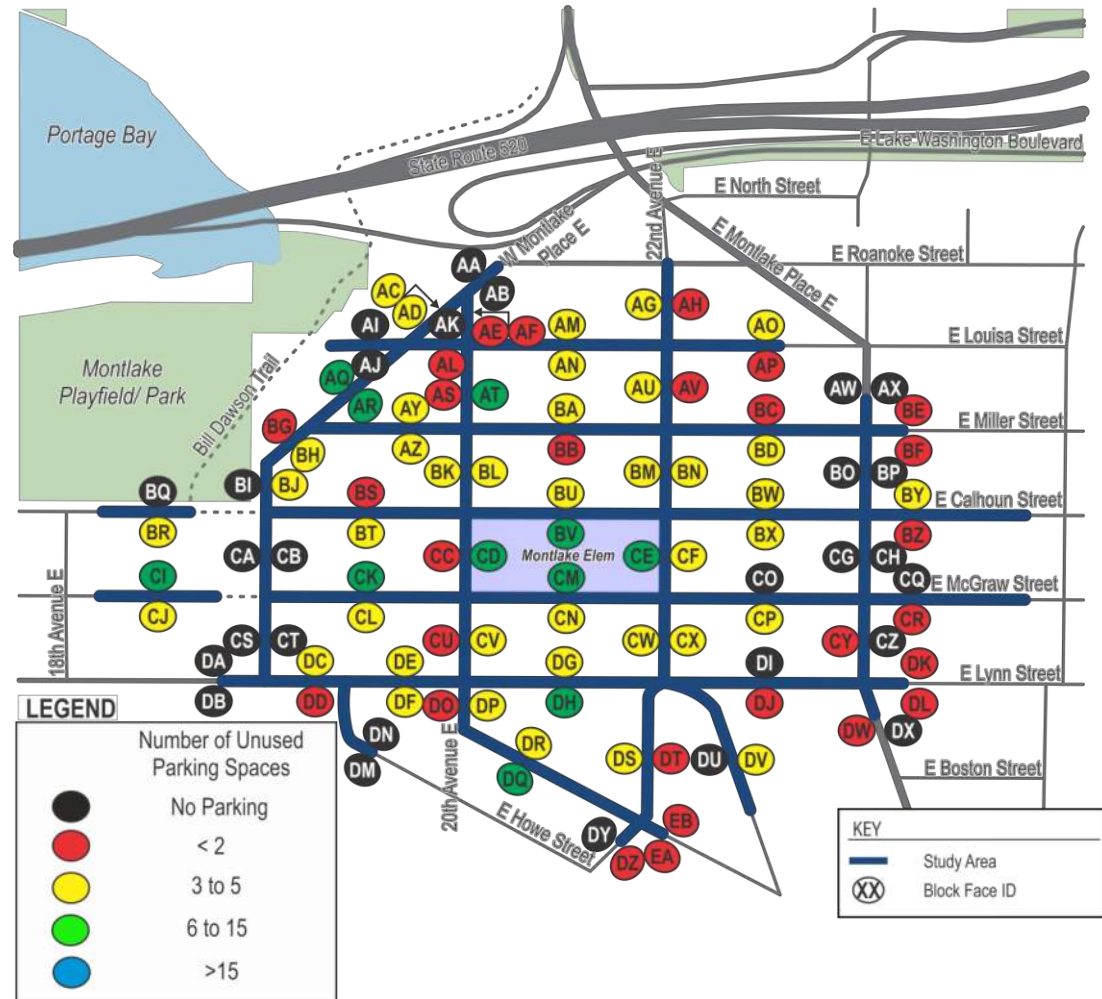
## On-Street Parking Occupancy - Weekday Evening (No Event)

Total On-Street Parking Spaces within 800-ft walking distance = 598 Spaces

- 332 to 342 cars (7:30 - 8:15 p.m.) (56% to 57% occupied)\*
- Number Unoccupied = 256 to 266

\* Note: City of Seattle considers occupancy rates of 85% or higher to be effectively full.

Source: *Montlake Elementary School Modernization and Addition Transportation Technical Report*, Heffron Transportation, Inc., 2022.



# Requested Departure #4 : Vehicular Parking Quantity

## Transportation and Parking Recommendations to Provide:

- **School Construction Transportation Management Plan:** The selected Contractor will develop this plan which addresses traffic and pedestrian control during construction of the new facility. It will define truck routes, lane closures, walkway closures, and parking or load/unload area disruptions, as necessary. To the extent possible, this plan will direct trucks along the shortest route to arterials and away from residential streets to avoid unnecessary conflicts with resident and pedestrian activity. This plan may also include measures to keep adjacent streets clean on a daily basis at the truck exit points (such as street sweeping or on-site truck wheel cleaning) to reduce tracking dirt offsite.
- **Develop Plan for Large-Events:** When enrollment reaches 300 students, for the one or two largest events each year (such as Curriculum Night), develop large-event plan to either: 1) identifies additional parking supply (such as parking at the Montlake Community Center to the northwest) and/or 2) modifies the event to reduce total peak demand by separating it into two sessions or into two nights based on grade levels (as occurs at some other Seattle elementary schools).
- **Develop Neighborhood Communication Plan for School Events:** Plan to inform nearby neighbors of large events (those expected to draw 500 people or more) each year.
- **Update right-of-way & curb-side signage:** Work with SDOT to confirm the locations, extents, and signage (such as times of restrictions) of the school-bus and/or school load zones established or eliminated on adjacent streets.

Source: *Montlake Elementary School Modernization and Addition Transportation Technical Report*, Heffron Transportation, Inc., 2022.



## Departure #4 Summary

The proposed design would require 131 on-site vehicle parking spaces per Seattle Municipal Code.

Seattle Public Schools proposes to provide 0 on-site vehicle parking spaces for a departure of 131 spaces.





Requested Departure #5:  
**Bus/Truck Loading & Unloading –  
Loading Berth**



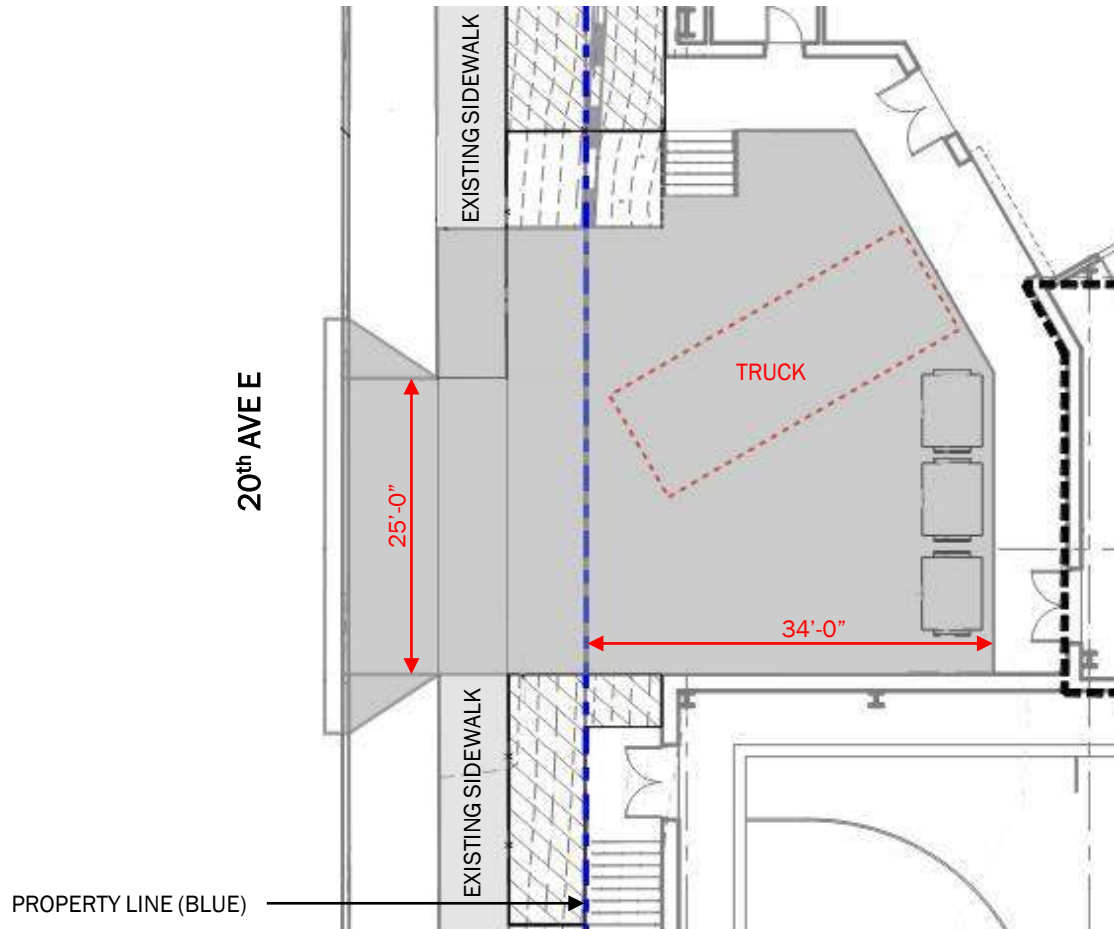
# Requested Departure #5 : Bus/Truck Loading & Unloading – Loading Berth

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
SMC 23.51B.002.I. – Bus and Truck Loading and Unloading

I.2. One off-street truck loading berth that is 13 feet wide and 40 feet long is required for new public school construction.

Departure Requested: Seattle Public Schools proposes a 25-foot wide by 34-foot long loading dock measured from the property line for a departure of 6’ below the standard loading berth length.

# Requested Departure #5 : Bus/Truck Loading & Unloading – Loading Berth



*Consistency:* Truck loading currently occurs at a small paved area accessed off of E Calhoun. This area does not provide adequate maneuvering clearance and trash pickup happens on the street. Proposal provides dedicated area for deliveries and trash pickup to occur off-street.

*Relationship:* The proposal provides a dedicated area to not only provide loading function, but also provide a sheltered area for loading activity as well as solid waste storage. This reduces odors, noise and visual impacts on the neighborhood and reduces traffic impacts.

*Need:* The limited site area available to fit educational program and the Owner's desire to maximize outdoor play space limit the available space for a service and loading dock area.

SPS proposes a 34'-0" deep loading berth accessed off 20<sup>th</sup> Ave E measured from the property line in lieu of the code-required 40'-0" depth. This configuration will accommodate the size of SPS vehicles anticipated to service the site.



## Departure #5 Summary

The Seattle Land Use Code requires one off-street truck loading berth that is 13 feet wide and 40 feet long is required for new public school construction.

Seattle Public Schools proposes one loading dock berth that is 25 feet wide by 34 feet long measured from the property line for a departure of 6' below the standard loading berth length.





Requested Departure #6:  
**Curb Cut to Service Area without Parking**



# Requested Departure #6 : Curb Cut to Service Area without Parking

Seattle Municipal Code Sections: SMC 23.54.030 – Parking Space and Access Standards  
SMC 23.54.030.F. – Curb Cuts

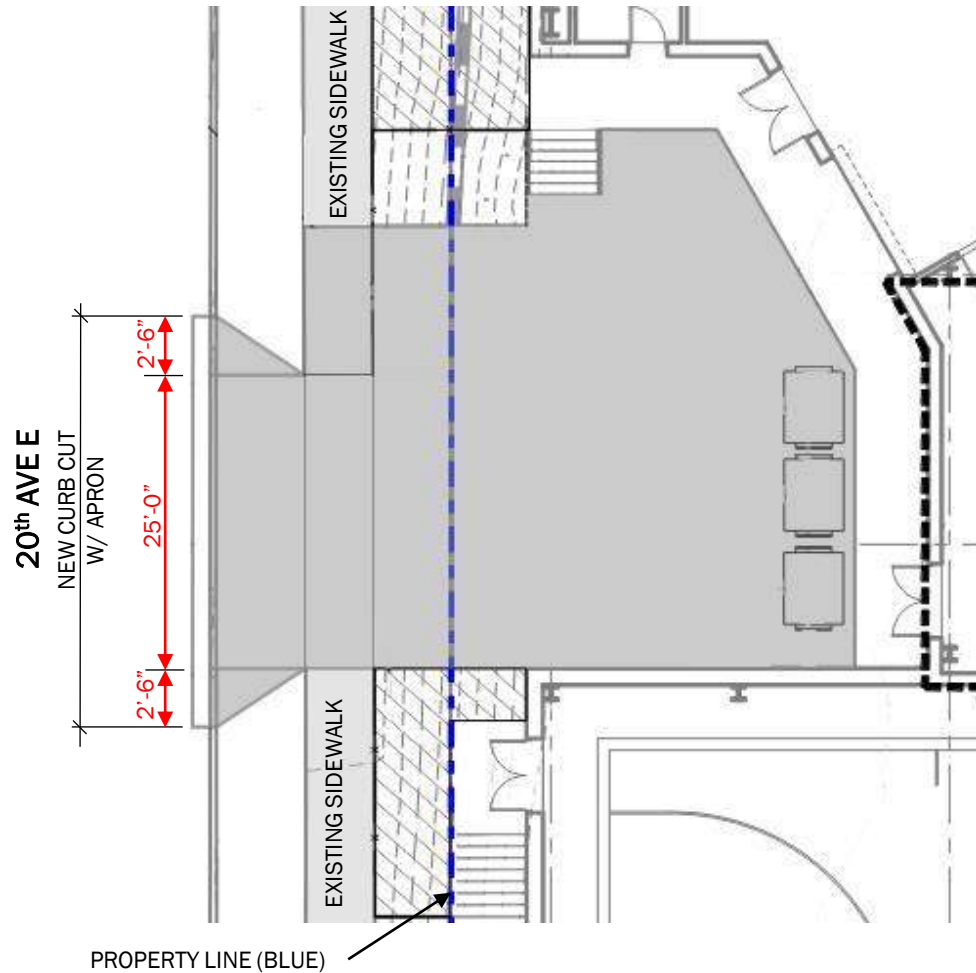
F.2.b.3. For public schools, the maximum width of a curb cut is 25 feet. Development standard departures may be granted or required pursuant to the procedures and criteria set forth in Chapter 23.79.

F.5. Curb cut flare. A flare with a maximum width of 2.5 feet is permitted on either side of curb cuts in any zone.

## Departure Requested:

Seattle Public Schools proposes a 25-foot wide curb cut with 2.5 foot flares for a departure to allow access to a service area without parking.

# Requested Departure #6 : Curb Cut to Service Area without Parking



*Consistency:* SPS anticipates approximately two street parking spaces will be lost to accommodate the new curb cut; however, two existing curb cuts along E Calhoun and E McGraw Streets will be re-established as part of the project, which may accommodate two additional street parking spaces, resulting in no loss of street parking spaces at the project site perimeter.

*Relationship:* A completed traffic study found that on-street parking within the site vicinity averages between 56% and 64% occupied on school days with between 201 and 266 unused spaces across the three observation periods. These levels are below the 85% threshold at which the City considers effectively full.

As noted in parking departure, the streets surrounding the project site require RPZ 1 parking permits and City-code allows employees of Montlake Elementary School to obtain RPZ 1 permits to park on-street in the vicinity. The increase in short-term parking associated with school visitors as well as increased staff parking could be accommodated by unused supply, and typical utilization is estimated to remain between 65% and 71%.

*Need:* The limited site area available to fit educational program and the Owner's desire to maximize outdoor play space limit the available space for a service and loading dock area.

SPS proposes a new 25'-0" curb cut with 2.5' flares along 20<sup>th</sup> Ave E to access a new service area without parking spaces. This configuration will accommodate all SPS service vehicles.



## Departure #6 Summary

The Seattle Land Use Code does not allow curb cuts to access service areas without parking spaces on site.

Seattle Public Schools proposes one 25 foot wide curb cut with 2.5 foot flares to each side for a departure to allow access to a service area without parking.





# Requested Departure #7: **Bicycle Parking (Long Term) Quantity**



# Requested Departure #7 : Bicycle Parking (Long Term) Quantity

Seattle Municipal Code Sections: SMC 23.54.015 Table D

Long-term bicycle parking spaces  
18 classrooms\* @ 3 spaces per classroom = 54  
1 per 4,000 SF @ Childcare = 1  
Total long-term spaces required = 55

\* The number of spaces required for classrooms is based on classrooms in the proposed new addition only.


Departure Requested:

The Seattle Land Use Code requires 55 total long-term bicycle parking spaces. Seattle Public Schools proposes 30 long-term bicycle parking spaces for a departure of 25 long-term parking spaces.

# Requested Departure #7 : Bicycle Parking (Long Term) Quantity

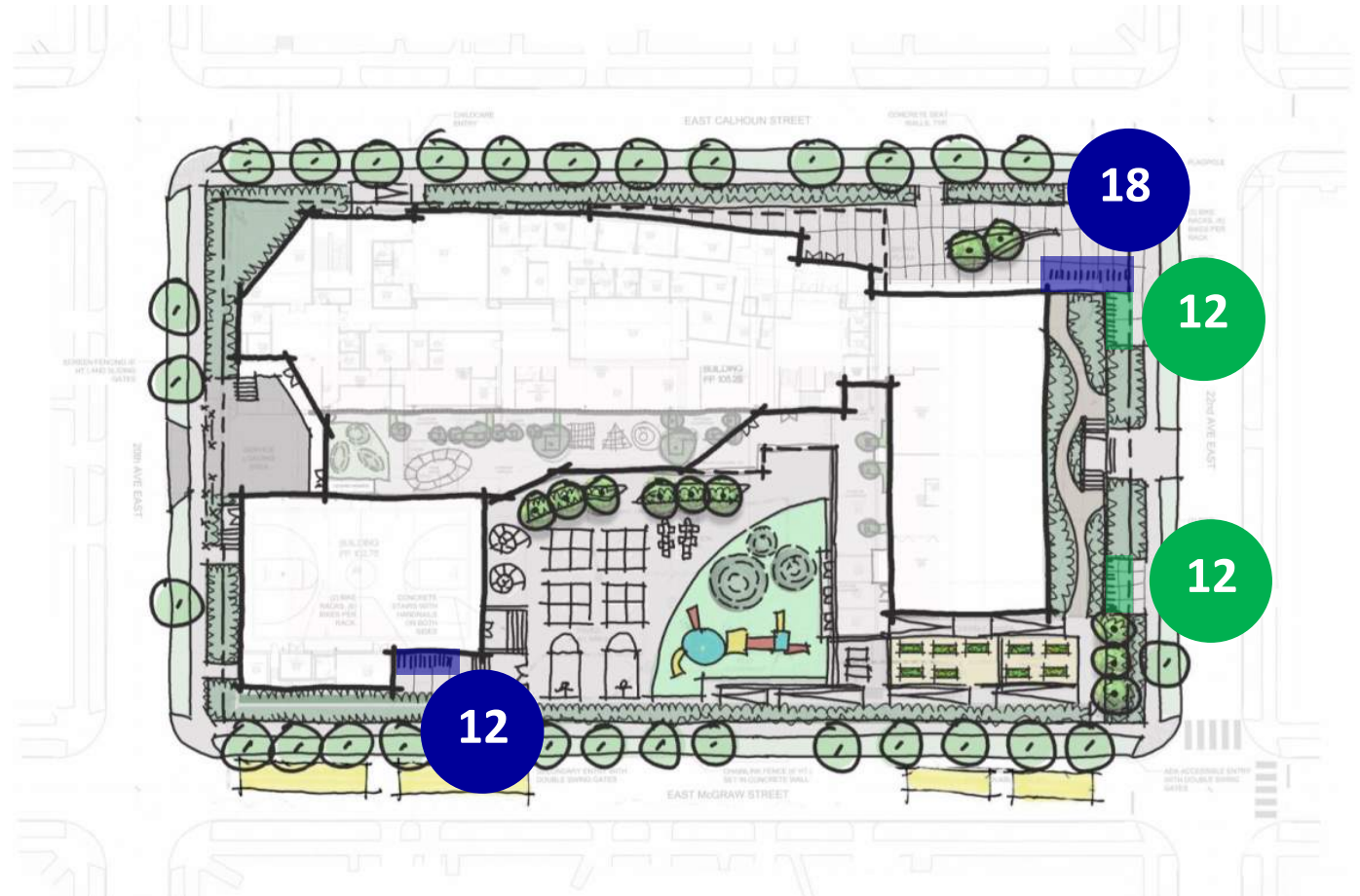
## Required Bicycle Parking Quantity per 23.54.015 Table D:

Long-Term Bicycle Spaces, based on 18 new addition classrooms: 55 total Long-Term spaces required:

 30 spaces provided, for a departure of 25 spaces

Short-Term Bicycle Spaces: based on 18 new addition classrooms: 20 total Short-Term spaces required:

 24 spaces provided (no departure required)





# Requested Departure #7 : Bicycle Parking (Long-Term) Quantity

## Long-Term Bicycle Parking

*Consistency:* The proposed design provides 30 long-term bicycle spaces. The current site does not provide any bike parking that meets the standards for long-term bike parking. A study conducted by school staff found between 3-5 students regularly bike to school each day and at most, 8 students bike to school each day representing approximately 4% of the existing student population. The proposed school design is intended to accommodate a student enrollment of 500, the 30 long-term bike parking spaces provided will accommodate 6% of the maximum enrollment. The proposed quantity brings the site closer to compliance with the code.

*Relationship:* The reduction in long-term bike parking is not anticipated to have any negative impacts on the surrounding community.

*Need:* The Montlake Elementary School site area is the second smallest in the District, at 72,000 SF or 1.65 acres. This small site area limits the available site square footage to accommodate long-term bicycle parking.

## Departure #7 Summary

The Seattle Municipal Code requires 55 long-term bicycle parking spaces for the proposed design.

Seattle Public Schools proposes 30 long-term bicycle parking spaces for a departure of 25 long-term parking spaces.



# Requested Departure #8: **Bicycle Parking Standards**



# Requested Departure #8 : Bicycle Parking Standards

Seattle Municipal Code Sections: SMC 23.51B.002 – Public Schools in Residential Zones  
SMC 23.51B.002.G. – Parking Quantity  
SMC 23.54 – Quantity and Design Standards for Access, Off-Street Parking, and Solid Waste Storage.  
SMC 23.54.015.K. Bicycle Parking

K.2. Performance standards. Provide bicycle parking in a highly visible, safe, and convenient location, emphasizing user convenience and theft deterrence, based on rules promulgated by the Director of the Seattle Department of Transportation that address the considerations in this subsection 23.54.015.K.2.

- a. Provide secure locations and arrangements of long-term bicycle parking, with features such as locked rooms or cages and bicycle lockers. The bicycle parking should be installed in a manner that avoids creating conflicts with automobile accesses and driveways.
- i. Provide full weather protection for all required long-term bicycle parking.

## Departure Requested:



The Seattle Land Use Code requires secure locations for all long-term bicycle parking with features such as locked rooms or cages and bicycle lockers. Seattle Public Schools proposes a secured enclosure for 12 of the 30 long-term spaces provided. Departure requested to allow open, weather protected bike racks for 18 long-term bike parking spaces.

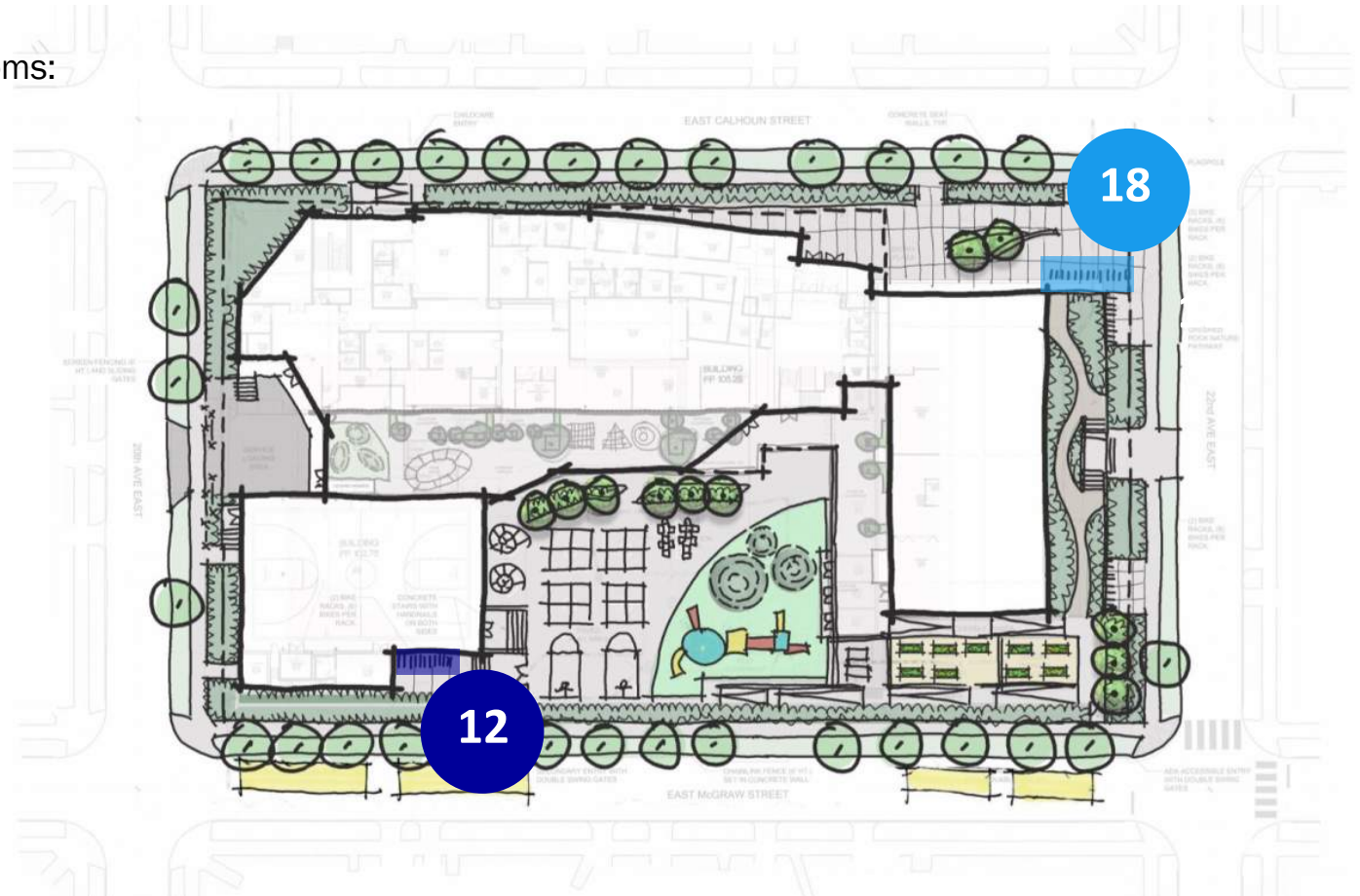


# Requested Departure #8 : Bicycle Parking Standards

## Long-Term Bicycle Parking Performance Standards:

Long-Term Bicycle Spaces, based on 18 new addition classrooms:  
55 total Long-Term spaces required, 30 spaces provided.

	Weather Protected / Secured	<b>12 spaces</b>
	Weather Protected / Not Secured	<b>18 spaces</b>



# Requested Departure #8 : Bicycle Parking Standards

## Long-Term Bicycle Parking Performance Standards

*Consistency:* The performance standard within the code is intended to provide bike parking that is in a visible and safe location to encourage use and deter risk of theft. The 12 provided long-term spaces are visible from the admin area which is staffed during school hours. All long-term parking provided does provide weather protection for bicycles.

*Relationship:* The requirement for locked bicycle rooms, cages or lockers is intended to apply to adult users who are issued keys or access cards to allow entry. This type of system is not practical for children in an elementary school environment.

*Need:* The Montlake Elementary School site area is the second smallest in the District, at 72,000 SF or 1.65 acres. This small site area limits the available site square footage to accommodate enclosed long-term bicycle parking for all of the spaces provided and does not have the capacity to administer access controls for younger children.



## Departure #8 Summary

The Seattle Land Use Code requires secure locations for all long-term bicycle parking with features such as locked rooms or cages and bicycle lockers.

Seattle Public Schools proposes a secured enclosure for 12 of the 30 long-term spaces provided. Departure requested to allow non-secured, weather protected bike racks for 18 long-term bike parking spaces.



Requested Departure #9:  
**Electric Changing-Image Message Board Sign**



# Requested Departure #9 : Electric Changing-Image Sign

Seattle Municipal Code Sections: SMC 23.55 – Signs  
SMC 23.55.020 - Signs in Single-Family Zones

B. No flashing, changing-image or message board signs permitted.

D. The following signs are permitted in all single-family zones:

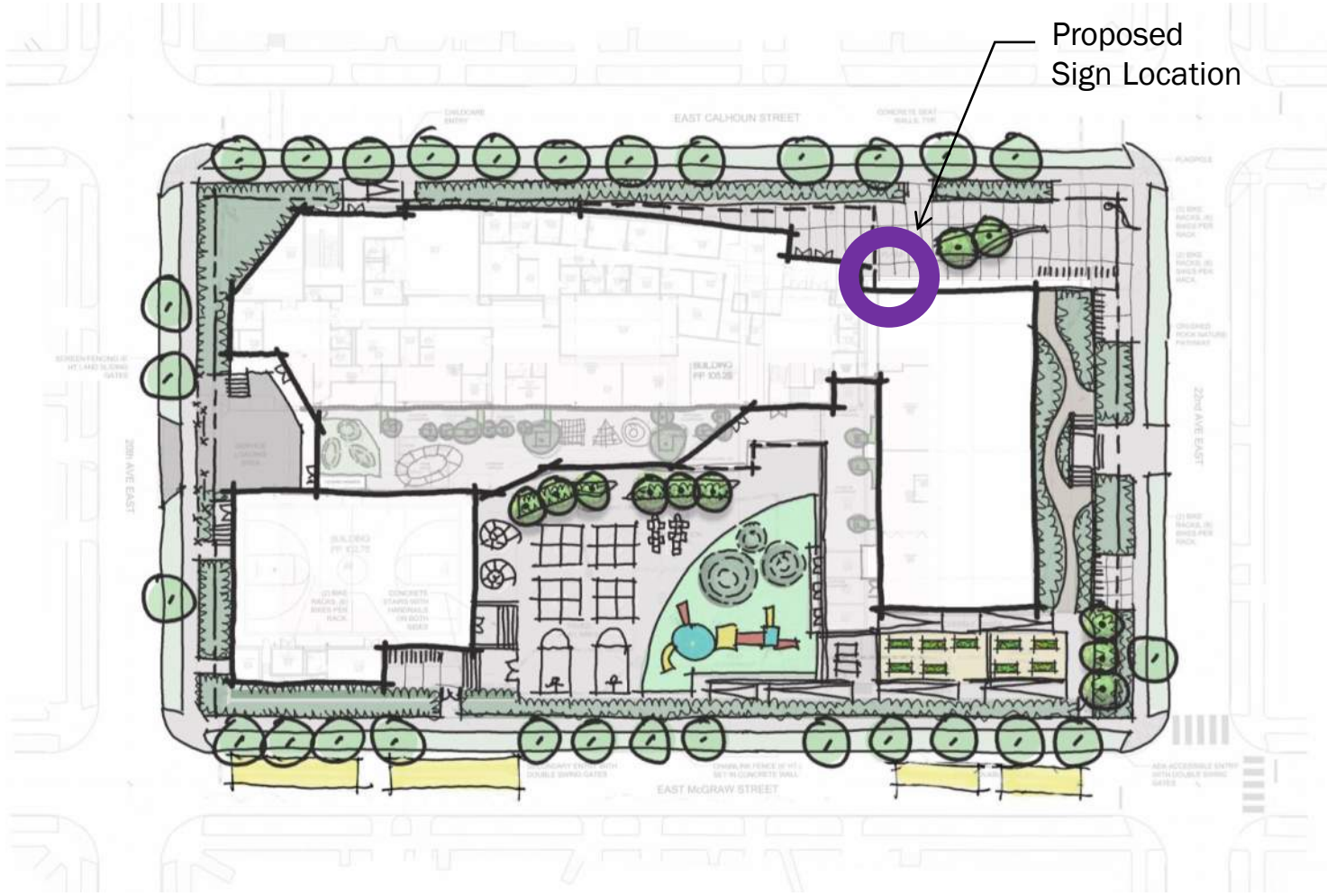
7. For elementary or secondary schools, one electric or nonilluminated double-faced identifying sign, not to exceed 30 square feet of area per sign face on each street frontage, provided that the signs shall be located and landscaped so that light and glare impacts on surrounding properties are reduced, and so that any illumination is controlled by a timer set to turn off by 10 p.m.

## Departure Requested:

The Seattle Land Use Code does not allow flashing, changing-image or message board signs. Seattle Public Schools requests a Departure to allow for (1) Changing Image Message Board Sign

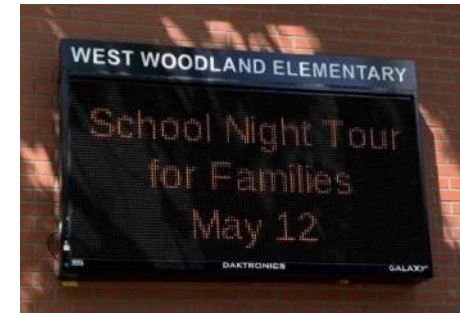
- a. The proposal is limited to one sign which may change images.
- b. The sign shall be set to turn on no earlier than 7am and turn off no later than 9pm; the sign may be used on weekends during these times.
- c. The sign is limited to be lit using one color with a dark background.
- d. No video, flashing, scrolling, tumbling or moving images allowed.

# Requested Departure #9 : Electric Changing-Image Sign



The proposed new main entrance location along East Calhoun Street will provide a secure entry for parents and visitors and work well with the parent pick up and drop off practices along Calhoun.

SPS would use the message board sign adjacent to the new main entrance to alert families and the community to events taking place at the school. Messages could be displayed in multiple languages, which a fixed message sign would not allow.



Sign Example



## Departure #9 Summary

The Seattle Land Use Code does not allow flashing, changing-image or message board signs in single-family residential zones.

Seattle Public Schools requests a departure to allow one changing image message board sign.



# Requested Departures Summary

**1. Departure for Lot Coverage: SMC 23.51B.002.C.2 and 3.**

Seattle Public Schools (SPS) requests a Departure to allow 45% lot coverage. (In addition, SPS requests that the director waive lot cover restrictions as allowed by SMC to permit an additional 12% for a total of 57% lot coverage.)

**2. Departure for Building Height: SMC 23.51B.002.D.1.c.**

SPS proposes a departure of 15' above the existing building height to accommodate mechanical and elevator equipment penthouses.

**3. Departure for Setbacks: SMC 23.51B.002, Table E**

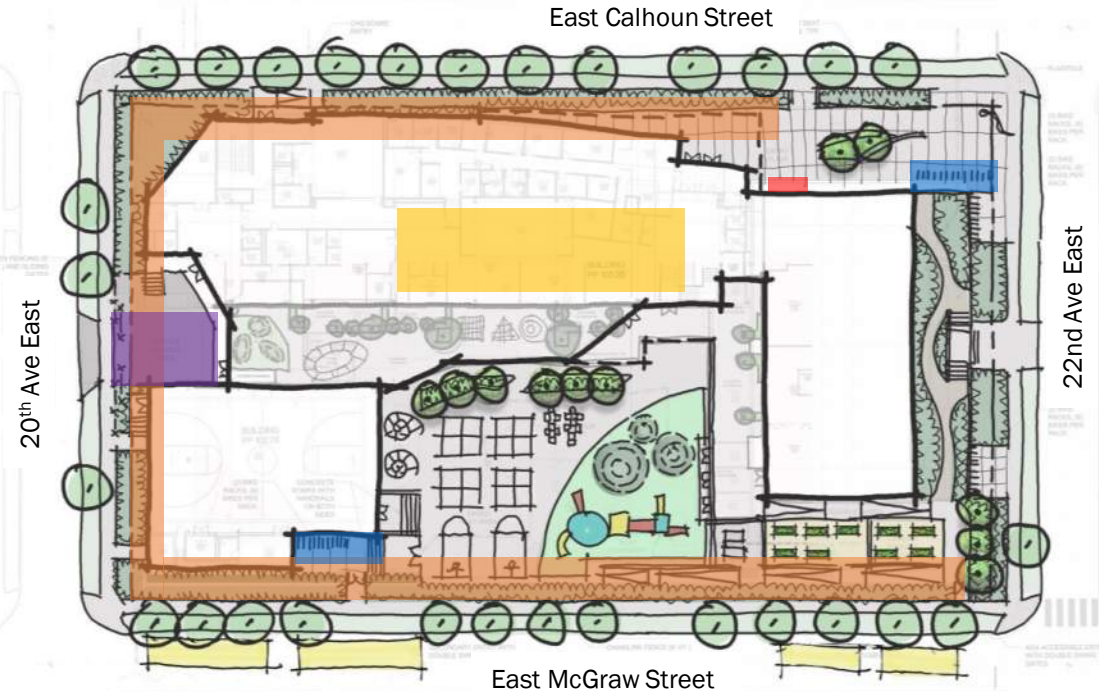
SPS proposes setbacks of 3 feet along the north property line, 6 feet along the west property line, and 5 feet along the south property line, for a departure of 12 feet along the north property line, 9 feet along the west property line, and 10 feet along the south property line.

**4. Departure for Vehicular Parking Quantity: SMC 23.54.015, Table C**

SPS proposes to provide 0 on-site vehicle parking spaces for a departure of 131 spaces.

**5. Departure for Bus & Truck Loading/Unloading: SMC 23.51B.002.I.2**

SPS proposes one loading dock berth that is 25 feet wide by 34 feet long measured from the property line for a departure of 6' below the standard loading berth length.



- Building Height
- Setbacks
- Bicycle Parking
- Changing Image Message Board Sign
- Bus/Truck Loading/Unloading



# Requested Departures Summary

- 6. Departure for Curb Cut to Service Area without Parking: SMC 23.54.030.F.2.b.3**  
SPS proposes one new 25 foot wide curb cut with 2.5 foot flares along 20th Ave E to allow access to a service area without parking.
  
- 7. Departure for Bicycle Parking (Long Term) Quantity: SMC 23.54.015 Table D:**  
SPS proposes 30 long-term bicycle parking spaces for a departure of 25 long-term bicycle parking spaces.
  
- 8. Departure for Bicycle Parking Standards: SMC 23.54.015.K.2.**  
SPS proposes a secured enclosure for 12 of the 30 long-term spaces provided.  
Departure requested to allow open, weather protected bike racks for 18 long-term bike parking spaces.
  
- 9. Departure for Changing Image Message Board Sign: SMC 23.55.020**  
SPS requests a departure to allow one changing image message board sign.

# Public Comment + Contact

Thank you for taking the time to review this document.

Please submit your comments on the requested departures, including any mitigation measures or conditions of approval by September 9, 2022 to:

Nelson Pesigan

[Nelson.pesigan@seattle.gov](mailto:Nelson.pesigan@seattle.gov)

City of Seattle, Department of Neighborhoods

Attn. Nelson Pesigan

PO Box 94649

Seattle, WA, 98124-4649